

#### McGOEY, HAUSER and EDSALL CONSULTING ENGINEERS D.P.C.

MARK J. EDSALL, P.E., P.P. (NY, NJ & PA) MICHAEL W. WEEKS, P.E. (NY, NJ & PA) MICHAEL J. LAMOREAUX, P.E. (NY, NJ, PA, VT & VA) MATTHEW J. SICKLER, P.E. (NY & PA) PATRICK J. HINES

Main Office 33 Airport Center Drive Suite 202 New Windsor, New York 12553

(845) 567-3100 fax: (845) 567-3232 e-mail: mheny@mhepc.com

Principal Emeritus: RICHARD D. McGOEY, P.E. (NY & PA) WILLIAM J. HAUSER, P.E. (NY, NJ & PA)

#### **TOWN OF NEWBURGH PLANNING BOARD TECHNICAL REVIEW COMMENTS**

PROJECT:

**U.S. CRANE & RIGGING** 

PROJECT NO.:

2016-14

**PROJECT LOCATION:** 

SECTION 97, BLOCK 1, LOT 21.2

REVIEW DATE:

10 MARCH 2017

**MEETING DATE:** 

16 MARCH 2017

PROJECT REPRESENTATIVE: MASER CONSULTING

- 1. The site has been redesigned to provide an emergency access only from Stewart Ave., this access will-be gated. Comments from Jurisdictional Fire Departmentregarding the access road should be received.
- 2. City of Newburgh Flow Acceptance letter for the new building must be received. Planning Board can take no action on the project until City of Newburgh Flow Acceptance letter is received. Proposed hydraulic loading from the structure should be identified in a narrative report submitted to the Town Engineer for processing with the City of Newburgh.
- 3. Crane and truck parking areas have been delineated on the site. The balance of the site will be utilized for storage of passenger vehicles by the current site operator. Expansion of the contractor yard/ LHI use beyond those depicted on the site plan would require review and approval by the Planning Board for future expansions.
- 4. Future submissions should incorporate architectural renderings for architectural review by the Planning Board.
- 5. A review of the proposed sanitary sewer lateral serving the new structure identifies that only the proposed office portion of the building will be served by the sanitary sewer line. It is unclear if restroom facilities and or other water uses are proposed in the larger portion of the structure. It is noted that the finished floor elevations between the office and the proposed building are approximately 11 ft elevation difference.
- 6. The Water Main Wet Tap detail identifies thrust blocks to be utilized. Town of Newburgh requires all piping be restrain joint pipe without the use of thrust blocks.

- 7. Sanitary sewer connection to an existing Town manhole must be by core drilling only, any other method is not acceptable to the Town. This must be noted on detail.
- 8. Gerald Canfield's comments regarding location of any proposed hydrants on the site should be received.
- 9. Stormwater Pollution Prevention Plan is under review by this office. The majority of the site is existing impervious cover. A slight reduction in impervious cover is proposed though implementation of landscaping and green areas. Water quality controls are proposed utilizing proprietary treatment products. A stormwater facilities control maintenance agreement is required to be executed.
- 10. The proposed structure is located at the side yard setback. Standard note requiring submission of surveyed plot plan prior to issuance of Building Permit should be added to the plan.
- 11. Previous comments regarding parking in front yard set back with regard to compliance with design guidelines still exist. The Applicants representatives have stated that landscaping is proposed to request a waiver for the design guideline requirement of no parking in front of structures. Planning Board should evaluate proposed landscape with regard to the need to grant the design guideline waiver.
- 12.A more detailed survey has been submitted with the plans. This office withdraws the comment requiring delineation of the NYSDEC Wetlands as topography on the site clearly indicates no work is proposed within wetland areas. Flood plain boundary has been depicted based on 2009 mapping and site topography.
- 13. Project requires Town Board approval for an overlay district.
- 14. Based on additional information submitted along with the detailed plans and Stormwater Pollution Prevention Plan identifying water quality control methodology this office would take no exception to the Planning Board issuing a Negative Declaration for the proposed project with the current scope of the project as indicated in the submitted narratives and plans.
- 15. It is requested the Applicants discuss with the Planning Board the storage of the cranes on the site. Visual impacts may result if numerous cranes are stored on the site in the operational mode for the cranes. Currently this office envisions the cranes being stored in a stowed condition and not with booms extended.

Respectfully submitted,

McGoey, Hauser and Edsall Consulting Engineers, D.P.C.

Patrick J. Hines Principal

PJH/kbw



Engineers
Planners
Surveyors
Landscape Architects
Environmental Scientists

555 Hudson Valley Ave., Suite 101 New Windsor, NY 12553

T: 845.564.4495 F: 845.567.1025

www.maserconsulting.com

March 3, 2017

#### **VIA HAND DELIVERY**

John Ewasutyn, Chairman Town of Newburgh Planning Board 308 Gardnertown Road Newburgh, New York 12550

MAR - / 201/

Re:

U.S. Crane, Newburgh

Town of Newburgh, Orange County, New York

MC Project No. 16001017A

Dear Chairman Ewasutyn:

Below please find our responses to comment letters received from Creighton Manning dated August 29, 2016 and January 27, 2017; and McGoey, Hauser & Edsall (MHE), dated August 24, 2016 and January 27, 2017. The comments have been repeated here for clarity.

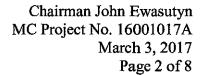
#### Creighton Manning August 29, 2016

Comment 1: The project proposes a new entrance to Stewart Avenue, about 200 feet north of Route 17K. The entrance is 30 feet wide. The applicant should confirm the intended use of this driveway as it serves a 20-vehicle parking lot and appears too small for truck/crane access.

Response 1: The entrance on Stewart Avenue, removed for a previous meeting with the Planning Board, has been replaced and changed to act as an access for emergency responders only. This was done to address the comments from the fire department officials during our public meeting on February 22<sup>nd</sup> 2017

Comment 2: How many employees and truck deliveries expected on a typical day?

Response 2: The initial estimates for employees are 30 people working a single shift. As explained at our initial meeting with the Planning Board and subsequently at our public meeting on 2/22/2017, cranes, or tractor-trailers with crane parts, normally leave the site early in the morning to miss rush-hour. A typical crane setup could take 10 or more tractor-trailers to erect. A typical operation would include four tractor-trailers leaving the site in one day to provide the necessary materials to begin building a crane. Similarly, when these tower cranes return to the site, they are disassembled and loaded on trucks which would return to the site in staggered increments. Truck cranes stored at the site could also be called upon periodically





and normally would require a flatbed vehicle to follow this type crane with counterweights and other equipment.

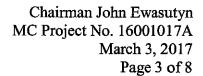
At most, three or four deliveries of steel would be anticipated any given day. Parts, Federal Express, UPS, garbage trucks would also make deliveries and pickups at the site.

It is not believed by the applicant or his Engineer that any of these vehicle motions would tax the four-lane highways serving the site (Route 17K and Route 300).

- Comment 3: The Route 17K entrance is about 70 feet wide, while the exit is about 40 feet. This access has historically accommodated delivery trucks, RV's, and trailers. Is this >100-foot wide access point necessary for the future use of the property? Can the width be reduced?
- Response 3: We agree with the Traffic Engineer's assessment of the driveway and as such, have reduced the width of the pavement. This reduction of pavement and increase in pervious surfaces has also benefitted the site in stormwater run- off reduction.
- Comment 4: Will oversized loads (weight, length, height, or width) be generated from the site?
- Response 4: Yes, oversized loads are a possibility considering the vehicles that are anticipated to use this site. The applicant, considering his business, is well-versed in the permitting requirements for operating these large vehicles on state highways.
- Comment 5: Where will steel deliveries (pick up or drop off) occur? Where/how will these vehicles circulate? Other than vehicles, will there be any outside storage of materials?
- Response 5: Steel deliveries are proposed to occur inside of the fabrication building. Architectural plans being prepared show interior cranes that will be utilized for off-loading the tractor-trailers that can drive into and out of the garage doors of this building. Circulation of these vehicles is shown on a separate drawing. Vehicles into the site from Route 17K can drive through the fabrication building, leave the site or park in one of over 50 spots designated for tractor-trailers or truck cranes.

Should there be a large delivery of steel it is possible that some of this material could be stored outside.

Comment 6: We would suggest that the sidewalk from Dollar General (BJS Holdings, Newburgh Project 2010-24) continue across the US Crane entrance to their westerly property line.





Response 6: We propose to extend the sidewalk from Dollar General across the entrance of the

site as requested.

Comment 7: Additional comments will follow with more detailed submissions.

Response 7: Comment noted.

#### Creighton Manning January 27, 2017

Comment 1: How many steel deliveries are expected in a typical day? Given the dispersion of the company's mobile cranes, how many, typically, would be at the Newburgh site? Are they used daily? Does their use fluctuate with the construction season? For tower cranes, we assume their use is long term (they are assembled on site and remain until the crane work is complete). How many tower crane truck deliveries (or trips) are generated each day? A narrative describing the number of US Crane employee, truck trips, and auto auction employee/delivery trips is suggested.

Response 1: We hope that our detailed response to Comment 2 above provides a sufficient response as to the typical operations of the site and the number of vehicles entering and leaving the site on a given day. The applicant will request data on the motions of vehicles into and out of the Auto Auction facility as requested.

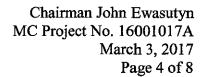
Comment 2: It's difficult to distinguish between what is existing and staying, versus what is existing and being removed/reconstructed. For example, is the crane trailer storage proposed within the existing fenced in lot? Is there a gate? How will they enter/exit? On a future submission, we suggest including a sheet that only shows proposed work and existing features that will remain after the site is developed.

Response 2: The initial sketch plan was not intending to show a high level of detail such as fencing, gates etc. This more detailed submittal should address the Engineer's request for more detailed site plan information.

Comment 3: Truck and trailer turning movements should be depicted where access and site travel is proposed.

Response 3: See above Comment 5 regarding circulation. Separate truck circulation plans have been provided with this application.

Comment 4: The Route 17K entrance is about 70 feet wide, while the exit is about 40 feet.





This access has historically accommodated delivery trucks, RV's, and trailers. Is this >100-foot wide access point necessary for the future use of the property? Can the width be reduced?

Response 4: See our Response 3 above.

Comment 5: Will oversize loads (weight, length, height, or width) be generated from the

Response 5: See our Response 4 above.

Comment 6: We would suggest that the sidewalk from Dollar General (BJS Holdings, Newburgh Project 2010-24) continue across the US Crane entrance to their westerly property line.

Response 6: See our response above regarding the sidewalk extension.

Comment 7: Additional comments will follow with more detailed submissions.

Response 7: Comment noted.

#### McGoey, Hauser & Edsall (MHE) August 24, 2016

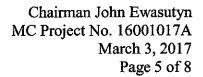
Comment 1: NYSDEC wetland boundaries should be updated on the plans. NYSDEC personnel should mark out the wetland boundary which should be surveyed by the Applicant's representative depicting current wetland boundary.

Response 1: We disagree with the Engineer's comment. There's absolutely no disturbance going on anywhere near the wetland on the site. There are many hundreds of feet of asphalt pavement in place between the proposed improvements and the wetland. We are not proposing any disturbance to the wetland or the buffer on this site and therefore we do not see a need to reestablish a wetland boundary where we are not near encroaching.

Comment 2: Approximate flood plain elevation depicted on the plans should be labeled in accordance with the 2009 flood plain maps. Actual flood plain boundaries should be depicted.

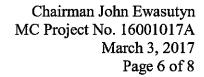
Response 2: Floodplain boundary has been shown on the plan and taken from the 2009 mapping.

Comment 3: Topography on the site should be depicted in support of the flood plain boundary and site development.



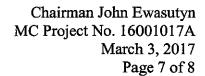


- Response 3: Land Surveyors from Maser Consulting attained the existing topography. Proposed site grading has been shown on the site.
- Comment 4: Finish floor elevations for existing and proposed structures should be labeled.
- Response 4: Floor elevations have been provided (see grading plan)
- Comment 5: City of Newburgh Flow Acceptance letter for increased flow from car/truck wash and proposed building should be provided.
- Response 5: The car/truck wash is an existing feature proposed to be continued to be used. Flows from the new building shall be provided and we shall provide estimates for the use of the car/truck wash.
- Comment 6: The narrative report identifies that stormwater quantity mitigation is not required as a re-development site. Treatment for water quality volumes will be required. Implementation of water quality control prior to discharge to the wetland area is required for re-developed sites.
- Response 6: Water quality improvements have been incorporated in the site plan as required for redevelopment sites. We have also sought wherever possible to increase the amount of pervious cover and the removal of areas previously paved.
- Comment 7: Access to Stewart Avenue should be submitted to the Highway Department for review. It is noted a 30-foot wide access drive is proposed. Are tractor-trailers proposed to access site from Stewart Avenue? It is noted that the drive lanes along the western portion of the proposed structure are only 20-foot width.
- Response 7: After meeting with the Fire Department, a gated emergency access is now proposed to access Stewart Avenue.
- Comment 8: Height of the building should be identified.
- Response 8: Exterior height of the building shall not exceed 35 feet. See the drawings by the applicant's Architect.
- Comment 9: Compliance with design guidelines regarding parking in front of the new proposed structure should be addressed. Site lighting should be depicted.
- Response 9: As the access to Stewart Avenue is now a gated emergency access only, we do not believe that this site violates the design guidelines regarding parking in front of a new proposed structure. We have proposed supplemental screening in this area. Site lighting plans have been provided with this application.





- Comment 10: Survey should depict adjoining land use and structures.
- Response 10: Land Surveyors for Maser consulting have provided a topographic and boundary survey including adjoining land uses and structures as requested.
- Comment 11: Gerry Canfield's input regarding the use of the site in the Ml zone should be received.
- Response 11: Based on Jerry Canfield's input at an earlier Planning Board Meeting, we are seeking the LH I overlay zone to be placed on this site. We have met with the Town Board and the Town Board has requested that we hold a meeting with the public which we did on 2/22/2017. Based on the turn-out at this public meeting and the lack of negative comments regarding this application, we sincerely hope that the Town Board acts swiftly in providing the LH I overlay zone for this site.
- Comment 12: Compliance with Section 109-18 Non Residential Structures in the flood plain should be addressed.
- Response 12: No new structures are proposed to be built in the floodplain. No fill is proposed to be placed in the floodplain. The only structures in the floodplain are existing structures which will require flood insurance.
- Comment 13: The number of cranes and trailers to be stored on the site should be identified.
- Response 13: The existing site has over 25 acres of pavement for vehicle storage. Fifteen (15) of these acres are proposed to be leased back to the Auto Auction for a period of five or six years. We have provided spaces for over 50 large vehicles on the site. Given the amount of pavement on the site, if any restrictions on the number of vehicles are to be imposed by the Planning Board, this will need to be reviewed by the applicant.
- Comment 14: Ken Wersted's comments regarding traffic flow on the site as well as onto Stewart Avenue and Route 17K should be addressed.
- Response 14: Responses to the Traffic Engineer's comments have been provided above. Traffic flow on the site as shown on the attached diagram's flow onto Stewart Avenue will only be for emergency vehicles and traffic to and from the site from Route 17K. This should not be an issue given the intensity of the existing and proposed uses on the site. The auto wash is a low volume user and the same applies for the proposed US Crane facility.
- Comment 15: The following agencies will be involved agencies for SEQRA review:
  - 1) NYSDOT





2) NYSDEC

The following will be interested agencies:

- 1) City of Newburgh
- 2) Orange County Planning
- Response 15: New York State DOT Permit shall be required. No permits are required from the New York State TEC and they need not be an involved agency as no encroachment or disturbance is proposed to the wetland buffer or wetland.
- Comment 16: The Applicant's representative is requested to advise the Board as to how the site will function with the multiple uses.
- Response 16: The applicant can discuss further with the Board his intention for use at the site and how the two businesses of a car/truck wash and US crane will utilize the site.

#### McGoey, Hauser & Edsall (MHE) January 27, 2017

- Comment 1: The Applicant is currently before the Town Board for a request to allow the heavy equipment overlay zone to be placed on the lot. Based on discussions with the Town Board, the access drive to Stewart Avenue has been eliminated. Project sponsors are to hold a Public Informational Meeting with surrounding residents.
- Response 1: Comment noted and other clarifications to this earlier comment have been provided in this document.
- Comment 2: The crane parking area has been moved to a more central portion of the site. Office area has been moved to the rear of the proposed structure where previously it was accessed from Stewart Avenue. Revised concept plan is being presented to the Board for a review, a single access point is proposed off NYS Route 17K at the existing access point of the project.
- Response 2: Again, noted and other clarifications to this earlier have been provided in this document. The office has been relocated to the south side of the building with no access propose to Stewart but for emergency access.
- Comment 3: Detailed review will be undertaken upon submission of complete design plans.
- Response 3: Comment noted



Chairman John Ewasutyn MC Project No. 16001017A March 3, 2017 Page 8 of 8

Very truly yours,

MASER CONSULTING P.A.

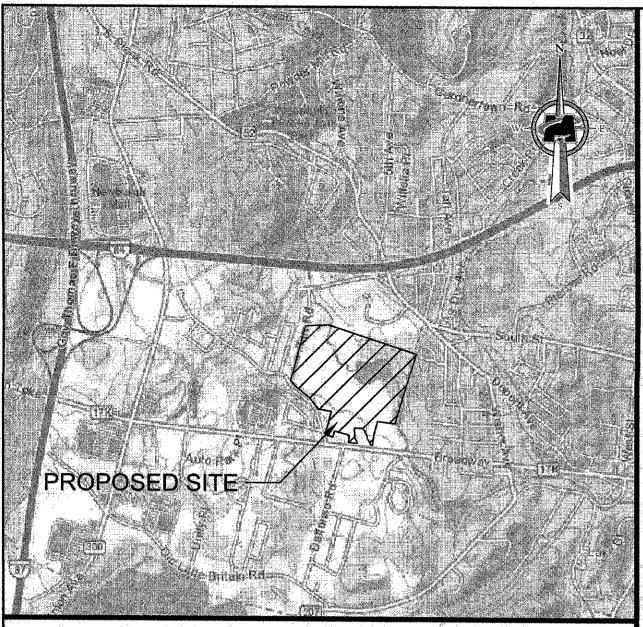
Andrew B. Fetherston, P.E.

Principal Associate

ABF/jm Enclosure

cc:

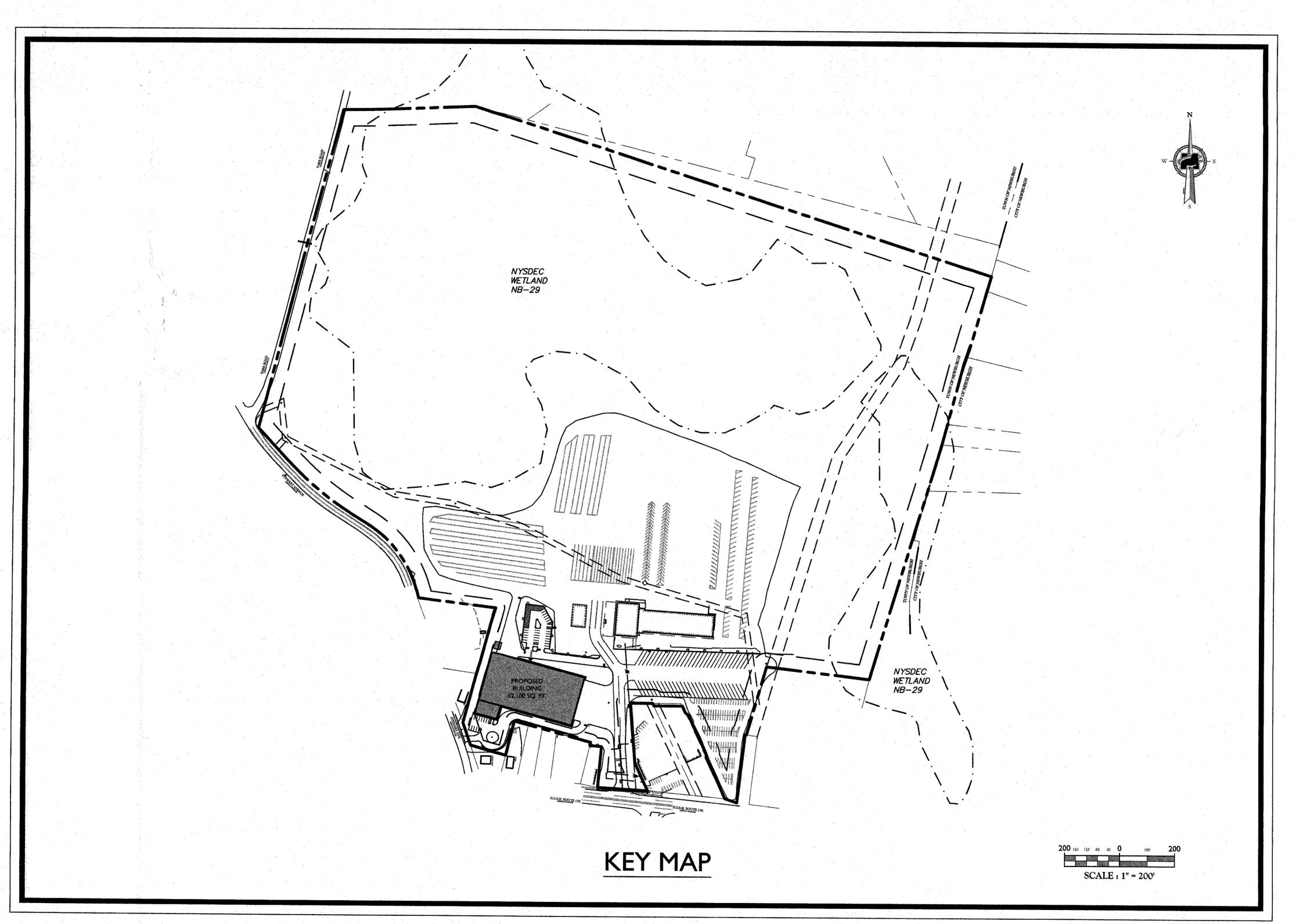
Tom Auringer, w/enclosures Tim McColgan, w/enclosures Verticon, LTD, w/enclosures



TAX MAP 1"=2000'

# PRELIMINARY SITE PLANS FOR 18 ROUTE 17K LLC SECTION 97, BLOCK 1, LOT 21.2 TOWN OF NEWBURGH

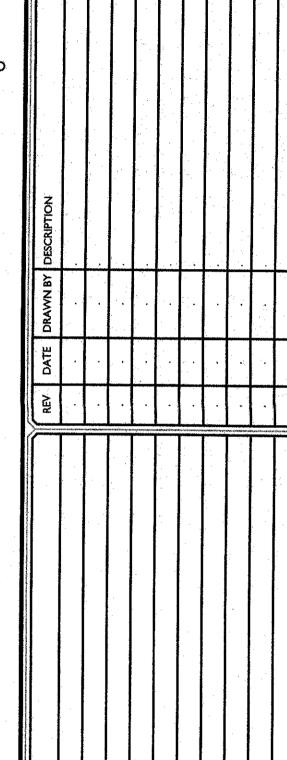
COUNTY OF ORANGE, STATE OF NEW YORK,



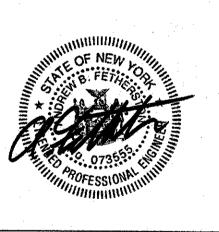
## GENERAL INFORMATION

- I. BOUNDARY AND TOPOGRAPHIC INFORMATION SHOWN HEREON IS TAKEN FROM A FIVE PAGE PLAN ENTITLED "BOUNDARY AND PARTIAL TOPOGRAPHIC SURVEY" PREPARED BY MASER CONSULTING, P.A. AND PREPARED FOR 18 ROUTE 17K, LLC, DATED JANUARY 19,
- 2. THE HORIZONTAL DATUM IS RELATIVE TO THE NEW YORK STATE PLANE COORDINATE SYSTEM EAST ZONE AND ADJUSTED TO NAD 1983. THE VERTICAL DATUM IS RELATIVE TO
- 3. THE 100 YEAR FLOOD PLAIN THAT IS ON SITE IS PER THE FLOOD INSURANCE RATE MAP 36071C0143E, DATED AUGUST, 2009 PREPARED BY THE FEDERAL EMERGENCY
- 4. MATERIALS, WORKMANSHIP, AND CONSTRUCTION FOR THE SITE IMPROVEMENTS SHOWN HEREON SHALL BE IN ACCORDANCE WITH:
  - A. NEW YORK STATE DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS", 2002; AS SUPPLEMENTED.
- B. CURRENT PREVAILING MUNICIPAL, COUNTY, AND/OR STATE AGENCY SPECIFICATIONS, STANDARDS, CONDITIONS, AND REQUIREMENTS.
   C. CURRENT PREVAILING UTILITY COMPANY/AUTHORITY SPECIFICATIONS, ST
- AND REQUIREMENTS.

  D. CURRENT MANUFACTURER SPECIFICATIONS, STANDARDS, AND REQUIREMENTS.
- 6. WATER SERVICE TO BE PROVIDED FROM THE EXISTING 12" WATER MAIN LINE ON STEWART AVENUE. WATER MAIN SIZE IS REFERENCED FROM INFORMATION PROVIDED BY THE TOWN OF NEWBURGH WATER DEPARTMENT. PROPOSED WATER MAIN EXTENSIONS AND FIRE HYDRANT LOCATIONS ARE SUBJECT TO MUNICIPAL REVIEW AND APPROVAL, AND AMERICAN WATERWORKS ASSOCIATION STANDARDS. PIPE MATERIALS SHALL BE CEMENT LINED DUCTILE IRON PIPE, CLASS 52, WITH ASPHALTIC EPOXY TYPE COATING. WATER MAINS SHALL BE INSTALLED TO PROVIDE A MINIMUM 4 FEET OF COVER FROM THE TOP OF PIPE TO THE PROPOSED GRADE.
- 7. SANITARY SEWER SERVICE SHALL BE PROVIDED BY GRAVITY EXISTING SEWER MAINS ON-SITE. THE GRAVITY MAIN WILL TIE INTO THE SEWER OWNED AN OPERATED BY THE TOWN SEWER DEPARTMENT. PROPOSED SEWER MAIN EXTENSIONS AND MANHOLE LOCATIONS ARE SUBJECT TO MUNICIPAL REVIEW AND APPROVAL, ACCORDING TO MUNICIPAL AND ORANGE COUNTY COUNTY HEALTH DEPARTMENT REGULATIONS. PIPE MATERIALS SHALL BE PVC SDR-35, EXCEPT AS NOTED OTHERWISE ON THE PLANS. EXCEPT WHERE SHALLOWER DEPTHS ARE PERMITTED BY THE MUNICIPALITY OR UTILITY AUTHORITY, SEWER LINES, INCLUDING FORCE MAINS AND LATERALS, SHALL BE INSTALLED TO PROVIDE A MINIMUM 4 FEET OF COVER FROM THE TOP OF PIPE TO PROPOSED GRADE.
- 8. SANITARY SEWERS AND STORM DRAINAGE PIPES SHALL BE SEPARATED FROM WATER MAINS BY AT LEAST 10 FEET HORIZONTALLY. IF SUCH SEPARATION IS NOT POSSIBLE, THE PIPES SHALL BE IN SEPARATE TRENCHES WITH THE SANITARY SEWER AT LEAST 18 INCHES BELOW THE BOTTOM OF THE WATER MAIN. WHERE APPROPRIATE SEPARATION IS NOT POSSIBLE, THE SEWER SHALL BE ENCASED IN CONCRETE, OR CONSTRUCTED OF DUCTILE-IRON-PIPE USING MECHANICAL OR SLIP ON JOINTS, FOR A DISTANCE OF 10 FEET ON EITHER SIDE OF THE CROSSING. IN ADDITION, ONE FULL LENGTH OF SEWER PIPE SHOULD BE LOCATED SO BOTH JOINTS ARE AS FAR AWAY AS POSSIBLE FROM THE WATER LINE.
- 9. GAS, ELECTRIC, LIGHTING, CABLE TELEVISION, AND ELECTRICAL SERVICE PLANS, IF REQUIRED, SHALL BE PREPARED BY THE RESPECTIVE UTILITY COMPANIES THAT SERVICE THE AREA PRIOR TO SITE CONSTRUCTION AND SHALL BE INSTALLED PER ORDINANCE REQUIREMENTS.
- 10. TELEPHONE, ELECTRIC, AND GAS LINES WILL BE INSTALLED UNDERGROUND. CROSSINGS OF PROPOSED PAVEMENTS WILL BE INSTALLED PRIOR TO THE CONSTRUCTION OF PAVEMENT BASE COURSE.
- 11. UTILITY RELOCATIONS SHOWN HEREON, IF ANY, ARE FOR INFORMATIONAL PURPOSES ONLY AND MAY NOT REPRESENT ALL REQUIRED UTILITY RELOCATIONS. THE CONTRACTOR IS RESPONSIBLE FOR PERFORMING AND/OR COORDINATING ALL REQUIRED UTILITY RELOCATIONS IN COOPERATION WITH THE RESPECTIVE UTILITY
- 12. STORM SEWERS SHALL BE CLASS III (OR HIGHER IF NOTED) REINFORCED CONCRETE PIPE (RCP) WITH "O" RING GASKETS, HIGH DENSITY POLYEHTYLENE PIPE (HDPE) OR APPROVED EQUAL AS NOTED. PROPER PIPE COVERAGE SHALL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION. PIPE LENGTHS SHOWN HEREON ARE FROM CENTER OF STRUCTURE
- 13. REFUSE AND RECYCLABLES SHALL BE AS SHOWN OF THE PLAN AND AS DEEMED ACCEPTABLE BY THE TOWN OF NEWBURGH.
- 14. TRAFFIC SIGNAGE AND STRIPING SHALL CORRESPOND TO THE MANUAL ON UNIFORM
- 5. THIS IS A SITE DEVELOPMENT PLAN AND UNLESS SPECIFICALLY NOTED ELSEWHERE HEREON, IS NOT A SURVEY.
- 6. BUILDING FOOTPRINT DIMENSIONS SHOWN HERON ARE APPROXIMATE. FINAL BUILDING FOOTPRINT DIMENSIONS FOR EACH BUILDING SHALL BE FURNISHED ON THE INDIVIDUAL PLOT PLANS/ARCHITECTURAL PLANS AT THE TIME OF APPLICATION FOR A BUILDING PERMIT. ALL STRUCTURES SHALL CONFORM TO THE APPROVED BULK ZONING REQUIREMENTS.
- 17. DO NOT SCALE DRAWINGS AS THEY PERTAIN TO ADJACENT AND SURROUNDING PHYSICAL CONDITIONS, BUILDINGS, STRUCTURES, ETC. THEY ARE SCHEMATIC ONLY, EXCEPT WHERE DIMENSIONS ARE SHOWN THERETO.
- 18. THIS SET OF PLANS HAS BEEN PREPARED FOR THE PURPOSES OF MUNICIPAL AND AGENCY REVIEW AND APPROVAL. THIS SET OF PLANS SHALL NOT BE UTILIZED AS CONSTRUCTION DOCUMENTS UNTIL ALL APPROVALS REQUIRED HAVE BEEN OBTAINED, ALL CONDITIONS OF APPROVAL HAVE BEEN SATISFIED AND THE DRAWINGS HAVE BEEN STAMPED "ISSUED FOR CONSTRUCTION". THIS SHALL INCLUDE APPROVAL OF ALL CATALOG CUTS, SHOP DRAWINGS AND/OR DESIGN CALCULATIONS AS REQUIRED BY THE PROJECT OWNER AND/OR MUNICIPALITY. CONSTRUCTION ON SITE SHALL BE IN COMPLIANCE WITH SITE PLANS APPROVED BY THE TOWN PLANNING BOARD.
- 19. EXISTING UNDERGROUND UTILITY INFORMATION SHOWN HEREON HAS BEEN COLLECTED FROM VARIOUS SOURCES AND IS NOT GUARANTEED AS TO ACCURACY OR COMPLETENESS. THE CONTRACTOR SHALL VERIFY ALL INFORMATION TO HIS SATISFACTION PRIOR TO EXCAVATION. WHERE EXISTING UTILITIES ARE TO BE CROSSED BY PROPOSED CONSTRUCTIONS, TEST PITS SHALL BE DUG BY THE CONTRACTOR PRIOR TO CONSTRUCTION TO ASCERTAIN EXISTING INVERTS, MATERIALS, AND SIZES. TEST PIT INFORMATION SHALL BE GIVEN TO THE ENGINEER PRIOR TO CONSTRUCTION TO PERMIT ADJUSTMENTS AS REQUIRED TO AVOID CONFLICTS. THE CONTRACTOR SHALL NOTIFY THE UNDER SIGNED PROFESSIONAL IMMEDIATELY IF ANY FIELD CONDITIONS ENCOUNTERED DIFFER MATERIALLY FROM THOSE REPRESENTED HEREON. SUCH CONDITIONS COULD RENDER THE DESIGNS HEREON INAPPROPRIATE OR
- 20. THE CONTRACTOR IS RESPONSIBLE FOR PROJECT SAFETY, INCLUDING PROVISION OF ALL APPROPRIATE SAFETY DEVICES AND TRAINING REQUIRED.
- 21. THESE GENERAL NOTES SHALL APPLY TO ALL SHEETS IN THIS SET.
- 22. INFORMATION SHOWN HEREON IS INCORPORATED WITH THE CONTENTS OF THE FOLLOWING REPORTS":
- "STORMWATER POLLUTION PREVENTION PLAN (SWPPP) PREPARED FOR 18 ROUTE 17K LLC, PREPARED BY MASER CONSULTING, P.A. DATED MARCH 2017.
   "LIMITED PHASE II REPORT/BUILDING B" PREPARED BY MASER CONSULTING P.A. FOR 18 ROUTE 17 K LLC, DATED OCTOBER 27, 2016.
- 4. PROPERTY DESCRIPTION:
- TAX LOT: 97-1-21.1
   LOT SIZE: 97.8 ACRES
- ZONE: IB (INTERCHANGE BUSINESS); SEEKING LHI OVERLAY
   PROPERTY LOCATION: 18 ROUTE 17K, NEWBURGH NY, 12550



FOR STATE SPECIFIC DIRECT PHONE NUMBERS VISI



ANDREW B. FETHERSTON
NEW YORK LICENSED PROFESSIONAL
ENGINEER - LICENSE NUMBER: 073555-1

PRELIMINARY SITE PLAN

FOR 18 ROUTE 17K LLC

SECTION 97 BLOCK 1 LOT 21.2

TOWN OF NEWBURGH COUNTY OF ORANGE STATE OF NEW YORK



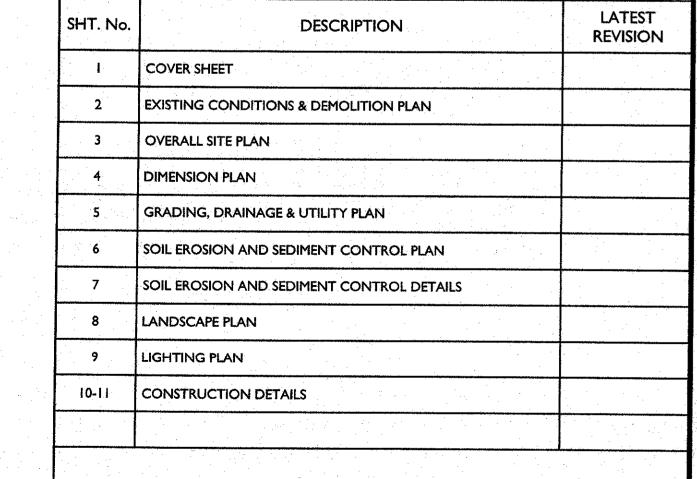
dson Valley Avenue Suite 101 Vindsor, NY 12553 ne: 845.564.4495 k: 845.567.1025

ALE: DATE: DRAWN BY: CHEC S SHOWN 03/01/17 CPM

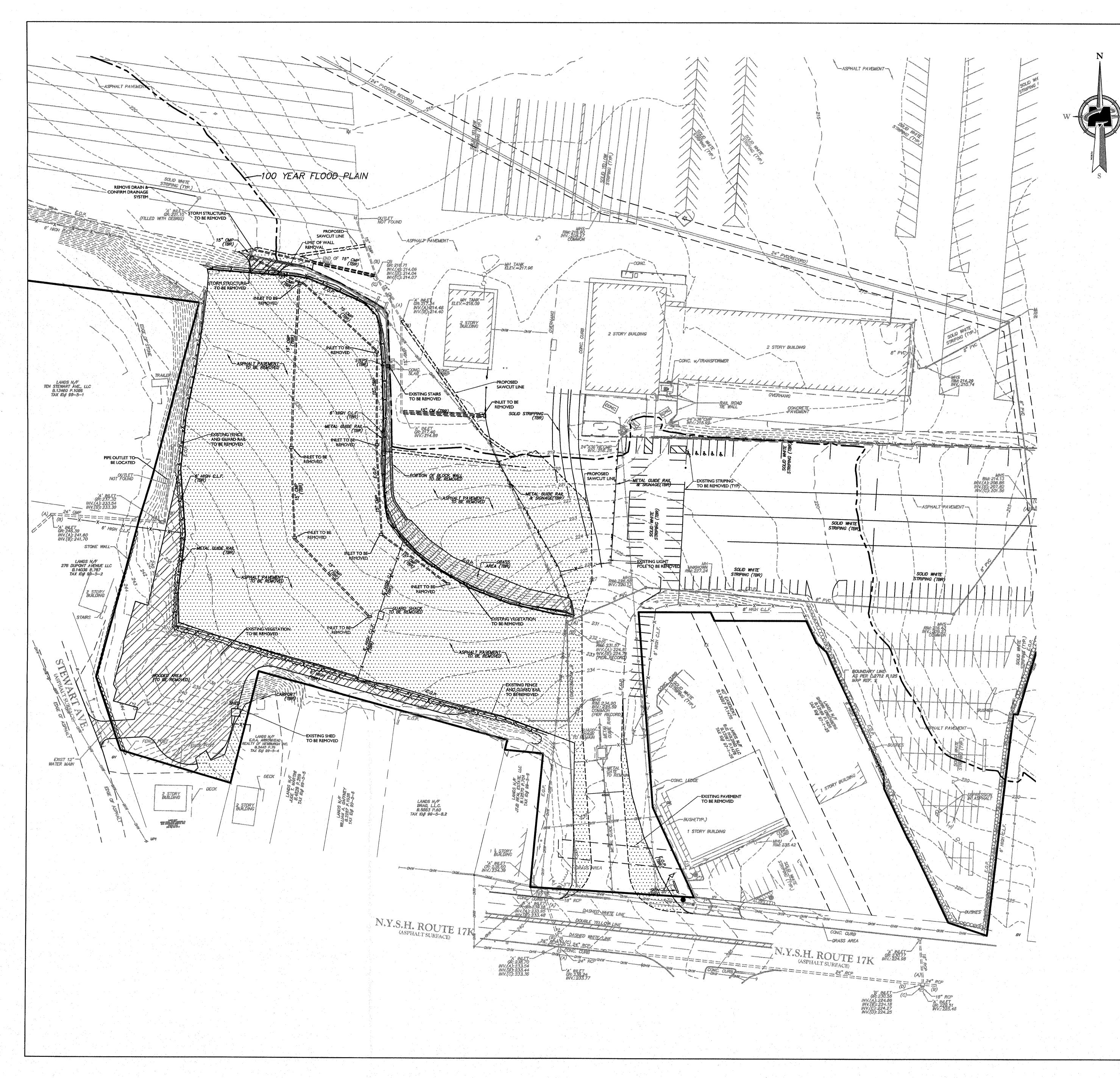
DJECT NUMBER: DRAWING NAME:
16001017A C-CVER

COVER SHEET

01 of 11



INDEX OF SHEETS



#### **DEMOLITION NOTES:**

- IN ACCORDANCE WITH STATE LAW, THE CONTRACTOR SHALL BE REQUIRED TO CALL THE BOARD OF PUBLIC UTILITIES ONE CALL DAMAGE PROTECTION SYSTEM FOR UTILITY MARK OUT IN ADVANCE OF ANY EXCAVATION. DIG SAFELY, NEW YORK, I-800-962-7962 OR 811. A PRE-DEMOLITION CONFERENCE WILL NEED TO BE ARRANGED WITH DIG SAFELY, NEW YORK AND MUST BE HELD A MINIMUM OF 7 DAYS BEFORE THE START OF ANY DEMOLITION.
- CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFYING ALL EXISTING SITE IMPROVEMENTS AND UTILITIES. ALL DISCREPANCIES SHALL BE IDENTIFIED TO THE ENGINEER IN WRITING.
- ALL DEMOLITION DEBRIS TO BE REMOVED BY CONTRACTOR IN ACCORDANCE WITH ALL APPLICABLE
- 4. MASER CONSULTING, P.A. IS NOT RESPONSIBLE FOR JOB SITE SAFETY OR SUPERVISION.
- ALL DEMOLITION ACTIVITIES ARE TO BE PERFORMED IN ACCORDANCE WITH THESE PLANS, AS WELL AS ALL FEDERAL, STATE AND LOCAL REGULATIONS. ANY DISCREPANCIES OR DEVIATIONS SHALL BE IDENTIFIED BY THE CONTRACTOR TO MASER CONSULTING, P.A. IN WRITING FOR RESOLUTION PRIOR TO INITIATION OF SITE ACTIVITY.
- 6. PRIOR TO STARTING ANY DEMOLITION CONTRACTOR IS RESPONSIBLE FOR/TO:

REMOVED DURING ALL DEMOLITION ACTIVITIES.

- A. ENSURING COPIES OF ALL PERMITS AND APPROVALS MUST BE MAINTAINED ON SITE AND AVAILABLE FOR REVIEW (SEE "EROSION AND SEDIMENT CONTROL PLAN," THIS DRAWING SET). B. INSTALLING THE REQUIRED SOIL EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO SITE
- C. ALL UTILITIES AND SERVICES, INCLUDING BUT NOT LIMITED TO GAS, WATER, ELECTRIC, SANITARY AND STORM SEWER, TELEPHONE, CABLE, FIBER OPTIC CABLE, ETC. WITHIN THE LIMITS OF DISTURBANCE. THE CONTRACTOR SHALL USE AND COMPLY WITH THE REQUIREMENTS OF THE APPLICABLE UTILITY NOTIFICATION SYSTEM TO LOCATE ALL THE UNDERGROUND UTILITIES. D. PROTECTING AND MAINTAINING IN OPERATION, ALL ACTIVE SYSTEMS THAT ARE NOT BEING
- E. FAMILIARIZING THEMSELVES WITH THE APPLICABLE UTILITY SERVICE PROVIDER AND IS RESPONSIBLE FOR ALL COORDINATION REGARDING PROVIDER AND IS RESPONSIBLE FOR ALL COORDINATION REGARDING UTILITY DEMOLITION REQUIRED FOR THE PROJECT. THE CONTRACTOR SHALL PROVIDE THE OWNER WRITTEN NOTIFICATION THAT THE EXISTING UTILITIES AND SERVICES HAVE BEEN TERMINATED AND ABANDONED IN ACCORDANCE WITH JURISDICTION AND UTILITY COMPANY F. CLEAN THE EXISTING UTILITY STRUCTURES ON-SITE PRIOR TO CONSTRUCTION AND VERIFY THE
- INVERTS FOR CONNECTION. 7. COORDINATION WITH UTILITY COMPANIES AND THE TOWN OF NEWBURGH REGARDING WORKING "OFF-PEAK" HOURS OR ON WEEKENDS AS MAY BE REQUIRED TO MINIMIZE THE IMPACT ON THE
- 8. A COMPLETE INSPECTION FOR CONTAMINANTS BY A LICENSED ENVIRONMENTAL TESTING AGENCY SHALL BE PERFORMED OF ALL BUILDINGS AND/OR STRUCTURES PRIOR TO REMOVAL SAME SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE LOCAL, STATE, AND FEDERAL ENVIRONMENTAL REGULATIONS. ANY/ALL CONTAMINANTS SHALL BE REMOVED AND DISPOSED OF BY A FEDERALLY LICENSED CONTRACTOR IN ACCORDANCE WITH FEDERAL, STATE AND LOCAL REGULATIONS. ALL ENVIRONMENTAL WORK INCLUDING HAZARDOUS MATERIAL, SOILS, ASBESTOS, OR OTHER REFERENCED

OR IMPLIED HEREIN IS THE SOLE RESPONSIBILITY OF THE OWNER'S ENVIRONMENTAL CONSULTANT.

- 9. MASER CONSULTING, P.A. IS NOT RESPONSIBLE FOR JOB SITE SAFETY OR SUPERVISION, CONTRACTOR IS TO PROCEED WITH THE DEMOLITION IN A SYSTEMATIC AND SAFE MANNER, FOLLOWING ALL THE O.S.H.A. REQUIREMENTS, TO ENSURE PUBLIC AND CONTRACTOR SAFETY.
- 10. THE CONTRACTOR SHALL PROVIDE ALL THE "MEANS AND METHODS" NECESSARY TO PREVENT MOVEMENT, SETTLEMENT, OR COLLAPSE OF EXISTING STRUCTURES, AND ANY OTHER IMPROVEMENTS THAT ARE REMAINING ON OR OFF SITE. THE DEMOLITION CONTRACTOR IS RESPONSIBLE FOR ALL REPAIRS OF DAMAGE TO ALL ITEMS THAT ARE TO REMAIN AS A RESULT OF HIS ACTIVITIES. ALL REPAIRS SHALL USE NEW MATERIAL. THE REPAIRS SHALL RESTORE THE ITEM TO THE PRE-DEMOLITION
- 11. TREE CLEARING ACTIVITIES CAN ONLY OCCUR BETWEEN THE DATES OF OCTOBER IST AND MARCH
- 12. THE CONTRACTOR SHALL PERFORM EARTH MOVEMENT ACTIVITIES, DEMOLITION AND REMOVAL OF ALL FOUNDATION WALLS, FOOTINGS, AND OTHER MATERIALS WITHIN THE LIMITS OF DISTURBANCE IN ACCORDANCE WITH DIRECTION BY OWNER'S STRUCTURAL OR GEOTECHNICAL ENGINEER.
- 13. ROCK EXCAVATIONS WILL BE PERFORMED BY MECHANICAL MEANS ONLY, USE OF EXPLOSIVES IS PROHIBITED. ALL THE REQUIRED PERMITS AND CONTROL MEASURES THAT ARE REQUIRED BY THE FEDERAL, STATE, AND LOCAL GOVERNMENTS SHALL BE IN PLACE PRIOR TO STARTING. THE CONTRACTOR IS ALSO RESPONSIBLE FOR ALL INSPECTION AND SEISMIC VIBRATION TESTING THAT IS REQUIRED TO MONITOR THE EFFECTS ON ALL LOCAL STRUCTURES, AS APPLICABLE.
- CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL AND GENERALLY ACCEPTED SAFE PRACTICES IN CONFORMANCE WITH: THE "MANUAL ON UNIFORM TRAFFIC CONTROL", AS WELL AS FEDERAL, STATE, AND LOCAL REGULATIONS WHEN DEMOLITION RELATED ACTIVITIES IMPACT ROADWAYS OR ROADWAY RIGHTS-OF-WAY.
- ROADS, STREETS, SIDEWALKS, WALKWAYS, AND OTHER ADJACENT FACILITIES. STREET CLOSURE PERMITS MUST BE RECEIVED FROM THE APPROPRIATE GOVERNMENTAL AUTHORITY.

15. CONDUCT DEMOLITION ACTIVITIES IN SUCH A MANNER TO ENSURE MINIMUM INTERFERENCE WITH

- 16. DEMOLITION ACTIVITIES AND EQUIPMENT SHALL NOT USE AREAS OUTSIDE THE DEFINED PROPERTY LINE WITHOUT WRITTEN PERMISSION OF THE OWNER, AND/OR APPROPRIATE GOVERNMENT AGENCY. 17. USE DUST CONTROL MEASURES TO LIMIT AIRBORNE DUST AND DIRT RISING AND SCATTERING IN THE
- AIR IN ACCORDANCE WITH FEDERAL, STATE, AND/OR LOCAL STANDARDS. AFTER THE DEMOLITION IS COMPLETE, ADJACENT STRUCTURES AND IMPROVEMENTS SHALL BE CLEANED OF ALL DUST AND DEBRI CAUSED BY THE DEMOLITION OPERATIONS. THE CONTRACTOR IS RESPONSIBLE FOR RETURNING ALL ADJACENT AREAS TO THEIR "PRE-DEMOLITION" CONDITION.
- 18. THIS DEMOLITION PLAN IS INTENDED TO IDENTIFY THOSE EXISTING ITEMS/CONDITIONS WHICH ARE TO BE REMOVED. IT IS NOT INTENDED TO PROVIDE DIRECTION OTHER THAN THAT ALL METHODS AND MEANS ARE TO BE IN ACCORDANCE WITH STATE, FEDERAL, LOCAL, AND JURISDICTIONAL REQUIREMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL O.S.H.A. AND OTHER SAFETY PRECAUTIONS NECESSARY TO PROVIDE A SAFE WORK SITE.
- WASTE) SHALL BE DISPOSED OF IN ACCORDANCE WITH ALL TOWN, COUNTY, STATE, AND FEDERAL LAWS AND APPLICABLE CODES.

19. DEBRIS SHALL NOT BE BURIED ON THE SUBJECT SITE, ALL DEMOLITION WASTES AND DEBRIS (SOLID

- 20. DEMOLITION SHALL NOT PROCEED UNTIL THE APPROXIMATE LOCATION OF THE EXISTING UTILITIES ARE MARKED IN THE FIELD AND ALL UTILITY CONNECTIONS ARE SUITABLY SHUT OFF AND DISCONNECTED AND PROPER DEMOLITION PERMITS ARE IN PLACE WITH THE TOWN.
- 21. CONTRACTOR IS RESPONSIBLE TO RESTORE ALL DISTURBED SITE AREAS TO ORIGINAL CONDITION AS
- 22. PROTECT ALL EXISTING UTILITIES TO REMAIN (INCLUDING DRAINAGE STRUCTURES, HYDRANTS, VALVES, SEWER MANHOLES, ETC.) DURING DEMOLITION. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGES CAUSED BY HIMSELF OR SUB-CONTRACTORS.

## DRAWING LEGENID

	DKAY	AIIAG FEGEIAD	
	PROPERTY BOUNDARY		
	EDGE OF PAVEMENT (E.O.P)	ALCONOMISSO TRANSPORTING TO THE PROPERTY OF TH	imacca:
	EXISTING CURB		18F/4811 P(25/000):
	DEPRESSED CURB	Account principle (1995)	rate adapt
	PAVEMENT STRIPING		
	OVERHEAD WIRES	OHWOHWOHW	
	WATERLINE MARKOUT	W	uman.
٠.	EASEMENTS		
	MAJOR CONTOUR	ndMANAMEMATURE management anterioristation confidences management and confidences confidences of	****
	MINOR CONTOUR	wooden moteratio submittee quantities spraycos enterprise prescribes submittee annihillate, pageston, o	man,
	SANITARY PIPES	restructuration of consequences in the consequence of the consequence	58 <b>5</b> 00
	STORMWATER PIPES	AMIN'NO DIRECTO ESTRUCTUR ARRIGINO MARIENO MARIENO MINISTER ARRICULTUR ARRICU	MANNO :
	TREELINE		mark f
			·
	CICAL		
	SIGN	· · · · · · · · · · · · · · · · · · ·	
	POST / BOLLARD		
	UTILITY POLE		
	GUY WIRE	spinore and the second	
	LIGHT POLE	<b>*</b>	
	SANITARY MANHOLE	<b>©</b>	
	SANITARY CLEANOUT	<b>o</b> .	
	DRAINAGE MANHOLE	<b>@</b>	

GR. = GRATE CO = CLEANOUT SAWCUT LINE PAVEMENT TO BE REMOVED

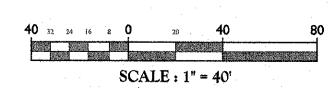
TBR = TO BE REMOVED UTP = UTILITY POLE DI = DRAIN INLET

CB = CATCH BASIN

WATER VALVE

CATCH BASIN

VEGETATION TO BE 211111111111



Engineers ■ Planners ■ Surveyors Landscape Architects # Environmental Scientist

Office Locations: ■ Albany, NY■ Chestnut Ridge, NY ■ New Windsor, NY Hamilton, NJ
Marmora, NJ
Mt. Arlington,
Mt. Laurel, NJ Lehigh Valley, PA
Exton, PA
Philadelphia, PA

Sterling, VA
Norfolk, VA ■ Albuquerque, NM State of N.Y. Certificate of Authorization: 0008671 / 00088 Copyright © 2017. Maser Consulting P.A. All Rights Reserved. This drawing and all the information contained herein is authorized for use only by the party for whom the services were contracted or to whom it is certified. This drawing may not be copied, reused, disclosed, distributed or relied upon for any other purpose without the express written consent of Maser Consulting, P.A.

PROTECT YOURSELF
ALL STATES REQUIRE NOTIFICATION OF
EXCAVATORS, DESIGNERS, OR ANY PERSON
PREPARING TO DISTURB THE EARTH'S
SUBSACE ANNOALIES IN ANY ETATE. Call before you dig. FOR STATE SPECIFIC DIRECT PHONE NUMBERS VISIT WWW.CALL811.COM

I											
ı											
I								ľ			
	empresi - in establish										
İ	The state of the s										
.	all a service and a service an			ļ.							
ı	ierk-referancies (Astronomisse										
										. , .	
ı	NOF									V .	
I	SCRIP	M									·
	DRAWN BY DESCRIPTION	-	Ŀ		•		l		•		
	\WN		•		٠			. /			
	DR	::									
	DATE		٠	•							
	REV						·	·	•		
		integrian services (except	· · · · · · · · · · · · · · · · · · ·		***************************************	***********	************		***************************************		$\overline{\bigcap}$
•	1										
I											
									÷		
										· .	
	NOIL										
	DESCRIPTION										
	WN BY DESCRIPTION										
	DRAWN BY DESCRIPTION										
	DATE DRAWN BY DESCRIPTION										



ANDREW B. FETHERSTON NEW YORK LICENSED PROFESSIONAL ENGINEER - LICENSE NUMBER: 073555-1

> **PRELIMINARY** SITE PLAN

18 ROUTE 17K LLC

BLOCK 1 LOT 21.2

SECTION 97

TOWN OF NEWBURGH COUNTY OF ORANGE STATE OF NEW YORK

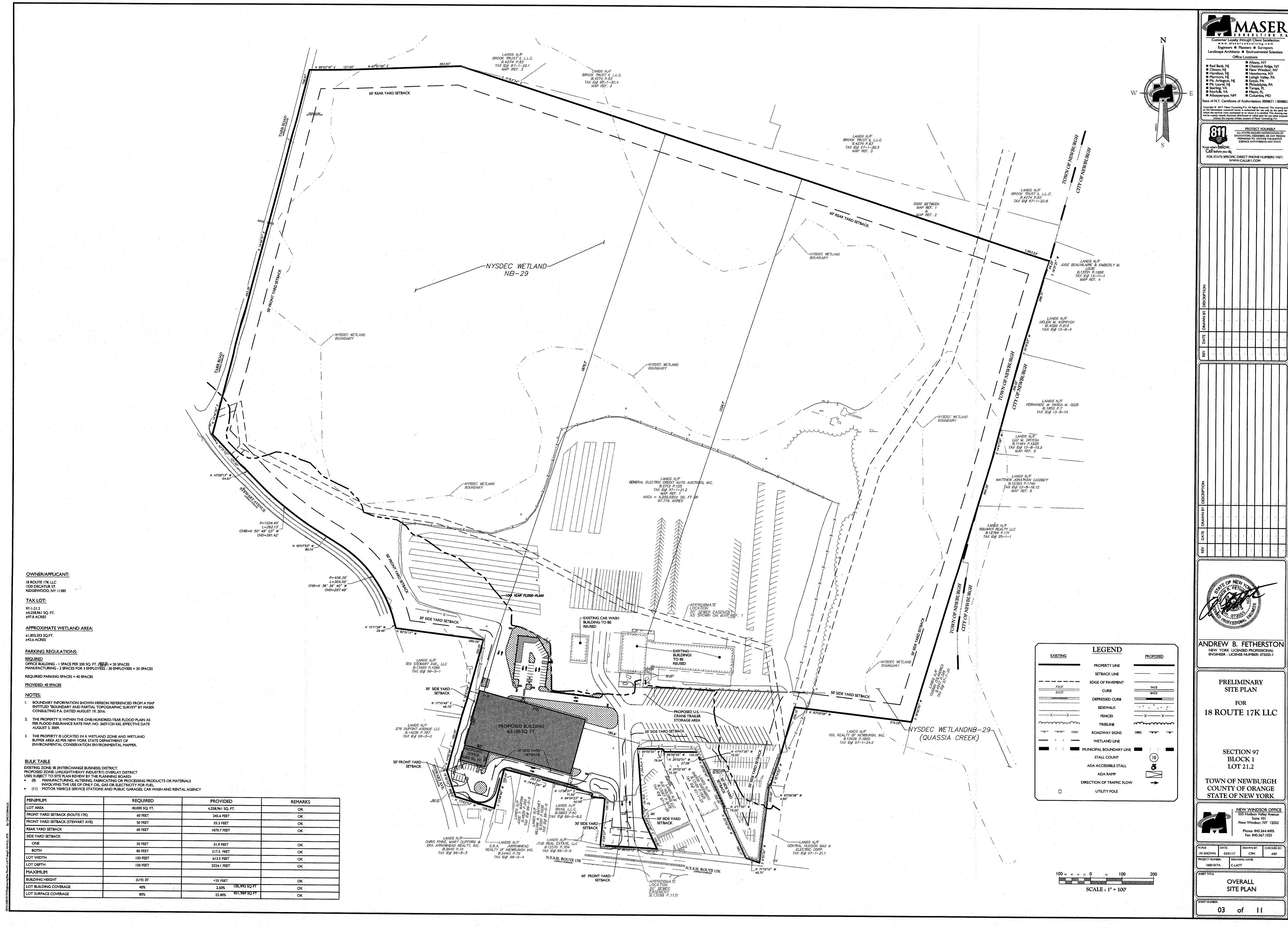


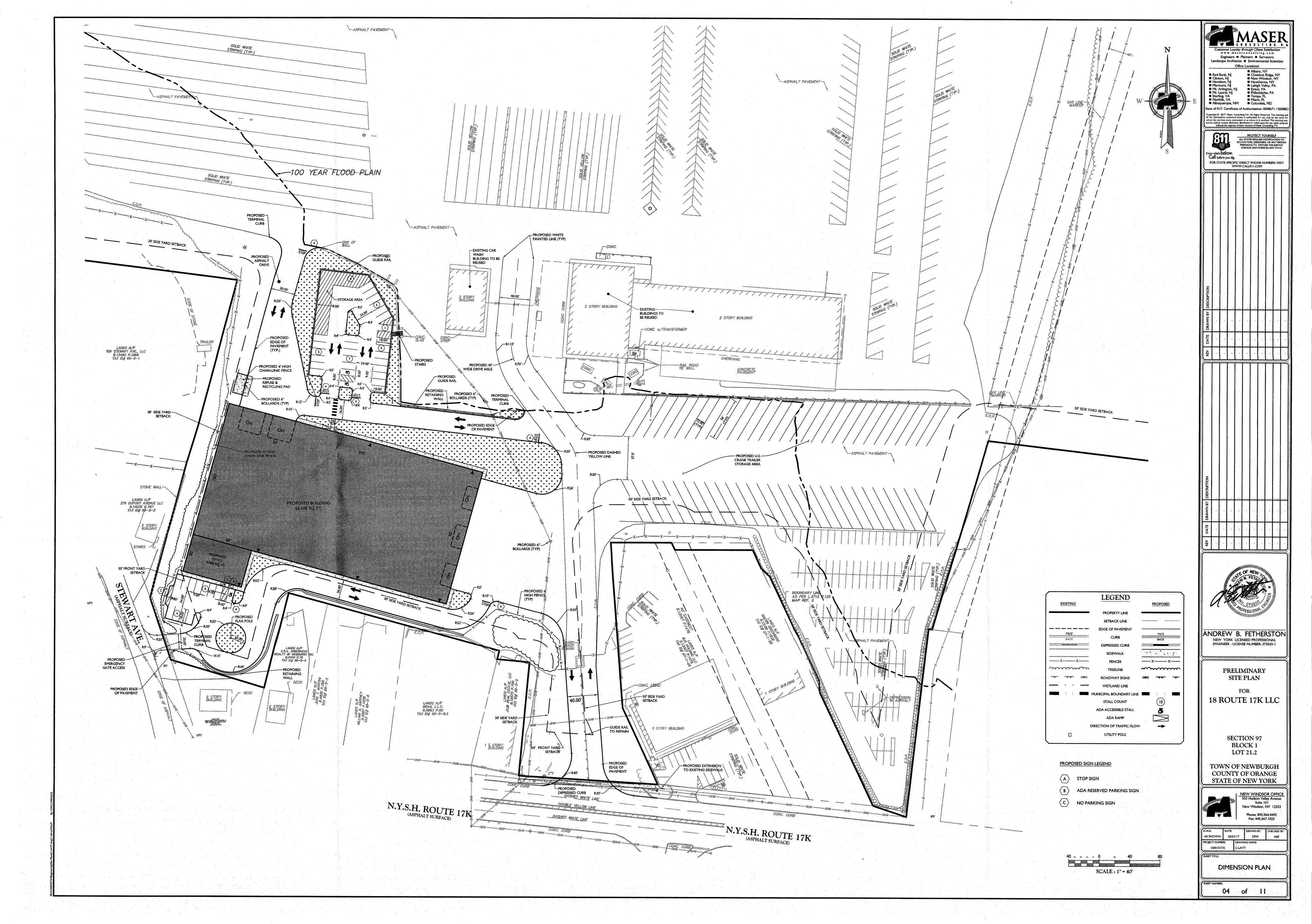
New Windsor, NY 12553 Phone: 845.564.4495 Fax: 845,567,1025

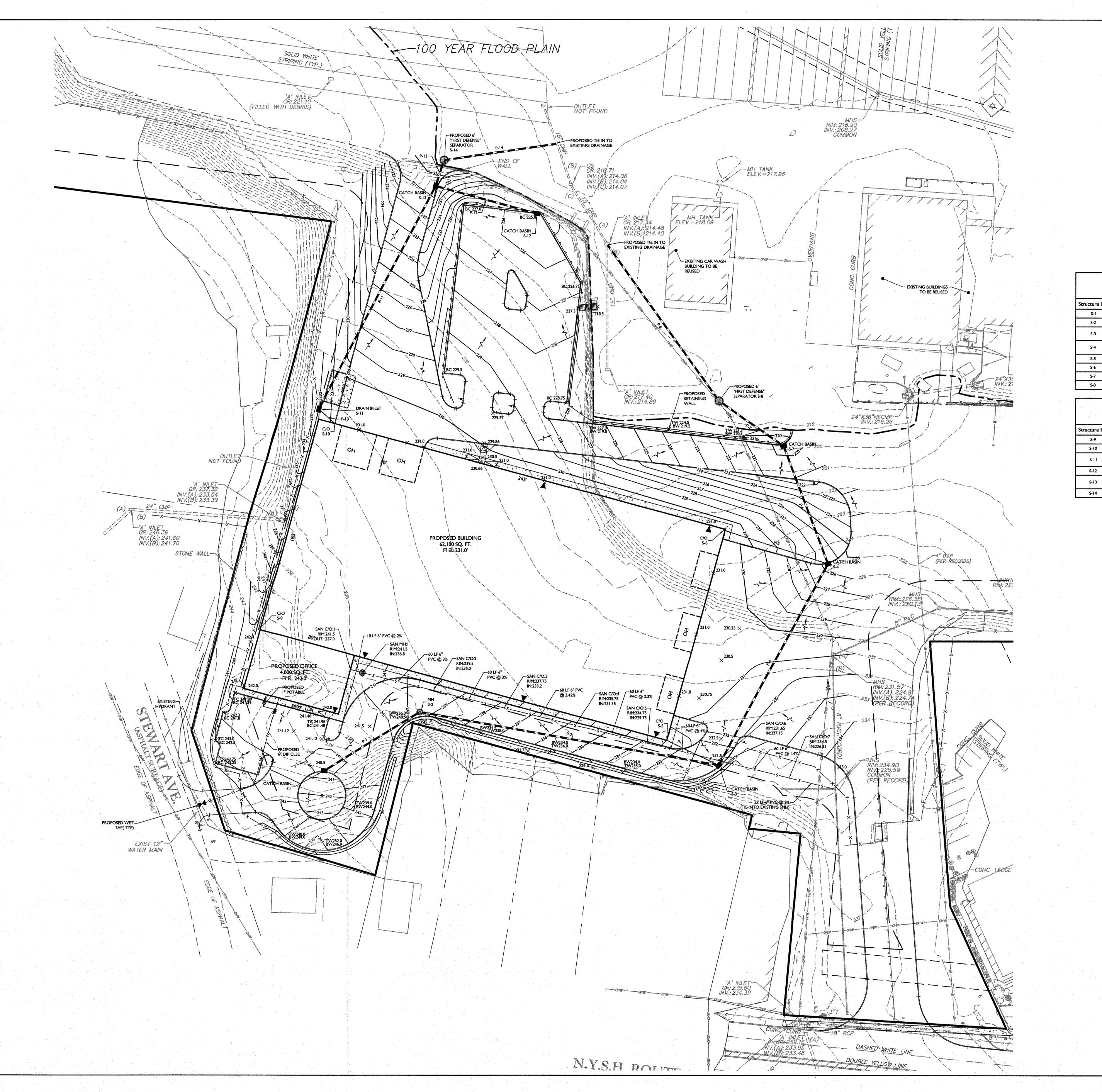
AS SHOWN 03/01/17 CPM

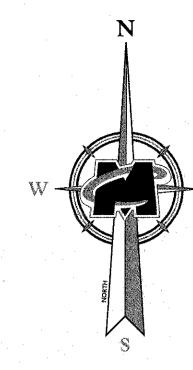
**EXISTING CONDITIONS &** DEMOLITION PLAN

02 of









			Drainag ire Tabl			
Structure I.D.	Description	Rim/Grate	Pipes (In)	Inverts (In)	Pipes (Out)	Inverts (Out)
S-I	30 in. x 48 in. 'A' INLET	240.50			15" HDPE	237.00
S-2	48 in. dia/CYLINDER MH	239.50	15" HDPE	235.00	15" HDPE	235.00
S-3	30 in. x 48 in. 'A' INLET	233.23	15" HDPE 4" HDPE	226.00 224.00	IS" HDPE	224.00
S-4	30 in. x 48 in. 'A' INLET	226.00	15" HDPE 4" HDPE	219.00 222.75	15" HDPE	218.75
S-5	6 in. Clean Out	234.00			4" HDPE	228.00
S-6	6 in. Clean Out	231.00			4" HDPE	228.50
S-7	30 in. x 48 in. 'A' INLET	219.50	15" HDPE	216.25	15" HDPE	216.25
S-8	72 in. dia. CYLINDER MH	219.36	15" HDPE	215.25	15" HDPE	215.25

			Drainag ire Tabl			
Structure I.D.	Description	Rim/Grate	Pipes (In)	Inverts (In)	Pipes (Out)	Inverts (Out)
S-9	6 in, Clean Out	240.86			4" HDPE	239.50
S-10	6 in, Clean Out	1.73			4" HDPE	227.75
S-11	30 in. x 48 in. 'A' INLET	232.00	4" HDPE 4" HDPE	227.00 227.75	15" HDPE	227.00
S-12	30 in. x 48 in. 'A' INLET	0.00			15" HDPE	221.50
S-13	30 in. x 48 in. 'A' INLET	227.00	15" HDPE 15" HDPE	217.20 217.25	15" HDPE	217.00
S-14	72 in. dia. CYLINDER MH	219.00	15" HDPE	215.75	15" HDPE	215.75

	EAST Drainage Pipe Table							
Pipe I.D.	Description	Length	Invert Up	Invert Dn	Slope			
P-1	15"HDPE	93'	237.00	235.00	2.16%			
्र P-2	15"HDPE	248'	235.00	226.00	3.63%			
P-3	4"HDPE	47	228.00	224.00	8.52%			
Å P-4	15"HDPE	188'	224.00	219.00	2.65%			
P-5	4"HDPE	90'	228.50	222.75	6.41%			
P-6	15"HDPE	104'	218.75	216.25	2.41%			
P-7	15"HDPE	65'	216.25	215.25	1.55%			
P-8	15"HDPE	157	215.25	214.40	0.54%			

	W		Drainage Table	<b>.</b>	
Pipe I.D.	Description	Length	Invert Up	Invert Dn	Slope
P-9	4"HDPE	188'	239.50	227.75	6.25%
P-10	4"HDPE	18,	227.75	227.00	4.28%
P-11	15"HDPE	207'	227.00	217.20	4.73%
P-12	15"HDPE	86'	221.50	217.25	4.93%
P-13	15"HDPE	22'	217.00	215.75	5.60%
P-14	15"HDPE	93'	215.75	-0.62	233.27%

			LEGEND			
	EXIST 但	A .	© TRAVERSE LINE, CENTER LINE O OR BASELINE (LABEL AS SUCH)		PROPOSED  13+0	ō.
			RIGHT OF WAY LINE		<del></del>	-
			PROPERTY LINE			-
	economic energica économic em		EDGE OF PAVEMENT			-
		CK	CURB		FACE BACK	=
			DEPRESSED CURB			=
·	washin tanih marana marana haranga mah	·····	SIDEWALK		A 4 . A.	-
	X	X	FENCES		×	-
	and the state of t	~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~	TREELINE	~	~~~~	\
	antilane male	manufactures manufactures	ROADWAY SIGNS			-
	<b>(53118</b> )	<b>*</b>	WETLAND LINE		• • •	-
	Being the state of		MUNICIPAL BOUNDARY LINE			
			'B' INLET			
		Construction of the Constr	'E' INLET			
		<u>D</u>	STORM MANHOLE		D	
		3)	SANITARY MANHOLE		<b>S</b>	
		<b>D</b>	FLARED END SECTION			
	E.	· · · · · · · · · · · · · · · · · · ·	HEADWALL			
	7	K	HYDRANT		<b>X</b>	
	,	\$	POLE MOUNTED LIGHT			
	resonance escential	es 75 anno man	* CONTOURS		75	-
	×ø	29.0	SPOT ELEVATION		<b>x</b> G 29.0	
			DIRECTION OF OVERLAND FLOV	<b>N</b>	<b>√</b> -	
	×n	29.0	TOP OF CURB ELEVATION		<b>★</b> TC 29.0	
	×B	C 29.0	BOTTOM OF CURB ELEVATION	I .	<b>★</b> BC 29.0	

30 24	18. 12	6 0	15	30
		2.50		
				See 1
4.5		SC	ALE: 1"	= 30'

	Cus	ww.	or Loy	aser	con	h Cli	ing.	I II atisfa	1	R. A.
State Copy aft the	Engineers Planners Surveyors  Landscape Architects Environmental Scientists  Office Locations:  Malbany, NY  Red Bank, NJ Chestnut Ridge, NY Chestnut Ridge, NY Hamilton, NJ Hawthorne, NY Hamilton, NJ Marmora, NJ Marmora, NJ Mt. Arlington, NJ Mt. Arlington, NJ Mt. Laurel, NJ Mt. Albuquerque, NM Mt. Albuquerque,									
Know	when the services were contracted or to whom it is certified. This drawing may not be copied, reused, disclosed, distributed or relied upon for any other purpose without the express written consent of Maser Consulting PA.  PROTECT YOURSELF ALL STATES REQUIRE NOTIFICATION OF EXCAVATORS, DESIGNERS, OR ANY PERSON PREPARING TO DISTURB THE EARTH'S SURFACE ANYWHERE IN ANY STATE  Know what's below. Call before you dig. FOR STATE SPECIFIC DIRECT PHONE NUMBERS VISIT: WWW.CALL811.COM									
										# 1994 (1994)
The service of the se										
DESCRIPTION										
DRAWN BY D										
REV DATE										
RIPTION										
DRAWN BY DESCRIPTION										
DATE				•					•	
~	ASS NO. 073555.									
AN	1DI	RE'	<b>V</b>	В.	FI	ΞΤΙ	ΗE	RS <sup>-</sup>	ГО	Z

PRELIMINARY SITE PLAN 18 ROUTE 17K LLC

**SECTION 97** 

BLOCK 1 LOT 21.2 TOWN OF NEWBURGH COUNTY OF ORANGE STATE OF NEW YORK

SCALE: DATE: DRAWN BY: CHECKED BY:
AS SHOWN 03/01/17 CPM ABF

GRADING, DRAINAGE AND UTILITY PLAN

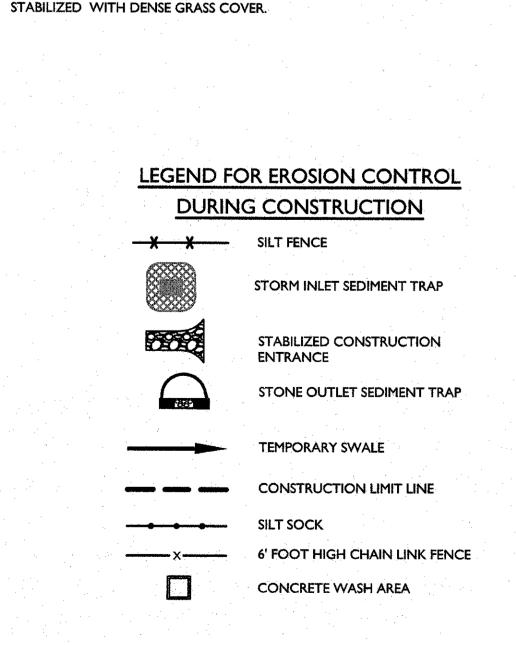
- I. ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE TO BE INSTALLED PRIOR TO ANY MAJOR SOIL DISTURBANCE, OR IN THEIR PROPER SEQUENCE, AND MAINTAINED UNTIL PERMANENT PROTECTION IS ESTABLISHED.
- 2. ANY DISTURBED AREAS THAT WILL BE LEFT EXPOSED MORE THAN FOURTEEN (14) DAYS FOR DISTURBANCES LESS THAN FIVE (5) ACRES AND SEVEN (7) DAYS FOR DISTURBANCES GREATER THAN FIVE (5) ACRES, AND NOT SUBJECT TO CONSTRUCTION TRAFFIC, WILL IMMEDIATELY RECEIVE A TEMPORARY SEEDING. IF THE SEASON PREVENTS THE ESTABLISHMENT OF TEMPORARY COVER, THE DISTURBED AREAS WILL BE MULCHED WITH STRAW, OR EQUIVALENT MATERIAL, AT A RATE OF TWO (2) TONS PER ACRE, ACCORDING TO STATE STANDARDS.
- . PERMANENT VEGETATION TO BE SEEDED OR SODDED ON ALL EXPOSED AREAS WITHIN FIVE (5) DAYS AFTER FINAL GRADING. MULCHING IS REQUIRED ON ALL SEEDING. WHEN HYDROSEEDING, MULCH SHALL NOT BE INCLUDED IN THE TANK WITH THE SEED.
- 4. ALL WORK TO BE DONE IN ACCORDANCE WITH THE LATEST VERSION OF THE NEW YORK STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL.
- 5. A SUBBASE COURSE WILL BE APPLIED IMMEDIATELY FOLLOWING ROUGH GRADING AND INSTALLATION OF IMPROVEMENTS TO STABILIZE STREETS, ROADS, DRIVEWAYS, AND PARKING AREAS. IN AREAS WHERE NO UTILITIES ARE PRESENT, THE SUBBASE SHALL BE INSTALLED WITHIN FIVE (5) DAYS OF THE PRELIMINARY GRADING.
- 6. IMMEDIATELY FOLLOWING INITIAL DISTURBANCE OR ROUGH GRADING, ALL CRITICAL AREAS SUBJECT TO EROSION (I.E. STEEP SLOPES AND ROADWAY EMBANKMENTS) WILL RECEIVE A TEMPORARY SEEDING IN COMBINATION WITH STRAW MULCH OR A SUITABLE EQUIVALENT, AT A RATE OF TWO (2) TONS PER ACRE, ACCORDING TO STATE STANDARDS.
- 7. ANY STEEP SLOPES RECEIVING PIPELINE INSTALLATION WILL BE BACKFILLED AND STABILIZED DAILY, AS THE INSTALLATION CONTINUES (I.E. SLOPES GREATER THAN 3:1).
- 8. THE STANDARD FOR STABILIZED CONSTRUCTION ACCESS REQUIRES THE INSTALLATION OF A STONE PAD, AT ALL CONSTRUCTION DRIVEWAYS, BEFORE INITIAL SITE DISTURBANCE.
- 9. IN ACCORDANCE WITH THE STANDARD FOR MANAGEMENT OF HIGH ACID PRODUCING SOILS, ANY SOIL HAVING A PH OF 4 OR LESS OR CONTAINING IRON SULFIDES SHALL BE COVERED WITH A MINIMUM OF TWELVE (12) INCHES OF SOIL HAVING A PH OF 5 OR MORE PRIOR TO SEEDBED PREPARATION. AREAS WHERE TREES OR SHRUBS ARE TO BE PLANTED SHALL BE COVERED WITH A MINIMUM OF TWENTY-FOUR (24) INCHES OF SOIL HAVING A PH OF 5 OR MORE.
- 10. AT THE TIME THE SITE PREPARATION FOR PERMANENT VEGETATIVE STABILIZATION IS GOING TO BE ACCOMPLISHED, ANY SOIL THAT WILL NOT PROVIDE A SUITABLE ENVIRONMENT TO SUPPORT ADEQUATE VEGETATIVE GROUND COVER, SHALL BE REMOVED OR TREATED IN SUCH A WAY THAT IT WILL PERMANENTLY ADJUST THE SOIL CONDITIONS AND RENDER IT SUITABLE FOR VEGETATIVE GROUND COVER IF THE REMOVAL OR TREATMENT OF THE SOIL WILL NOT PROVIDE SUITABLE CONDITIONS, NON-VEGETATIVE MEANS OF PERMANENT GROUND STABILIZATION WILL HAVE TO BE EMPLOYED.
- 11. CONDUIT OUTLET PROTECTION MUST BE REVIEWED & SUPPLEMENTED AT ALL OUTFALLS PRIOR TO THE DRAINAGE SYSTEM BECOMING OPERATIONAL.
- 12. UNFILTERED DEWATERING IS NOT PERMITTED. TAKE ALL NECESSARY PRECAUTIONS DURING ALL DEWATERING OPERATIONS TO MINIMIZE SEDIMENT TRANSFER. ANY DEWATERING METHODS USED MUST BE IN ACCORDANCE WITH STATE STANDARDS.
- 13. SHOULD THE CONTROL OF DUST AT THE SITE BE NECESSARY, THE SITE WILL BE SPRINKLED UNTIL THE SURFACE IS WET, TEMPORARY VEGETATIVE COVER SHALL BE ESTABLISHED OR MULCH SHALL BE APPLIED IN ACCORDANCE WITH STATE STANDARDS FOR EROSION CONTROL.
- 14. ALL SOIL WASHED, DROPPED, SPILLED, OR TRACKED OUTSIDE THE LIMIT OF DISTURBANCE OR ONTO PUBLIC RIGHT-OF-WAY WILL BE REMOVED IMMEDIATELY.
- 15. THE PROPERTY OWNER SHALL BE RESPONSIBLE FOR ANY EROSION AND SEDIMENTATION THAT MAY OCCUR BELOW STORMWATER OUTFALLS OR OFFSITE AS A RESULT OF CONSTRUCTION OF THE PROJECT.
- 16. STOCKPILE AND STAGING LOCATIONS DETERMINED IN THE FIELD, SHALL BE PLACED WITHIN THE LIMITS OF DISTURBANCE ACCORDING TO THE CERTIFIED PLAN.
- 17. CONCRETE WASHOUT, DUMPSTER, & STAGING AREA LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED AT THE PRE-CONSTRUCITON MEETING. THEY SHALL BE PLACED IN THE PROXIMITY OF THE CONSTRUCTION ENTRANCE AND STAGING AREAS AND SHALL BE USED PRIOR TO EXITING THE PROJECT SITE. THE LOCATION SHALL BE IN A PRACTICAL, CLEARLY DELINEATED, AREA AND BE MAINTAINED THROUGHOUT CONSTRUCTION.
- 18. ALL SOIL STOCKPILES ARE TO BE TEMPORARILY STABILIZED IN ACCORDANCE WITH THE LATEST VERSION OF THE NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL.
- 19. IF SOIL ACTIVITIES HAVE BEEN TEMPORARILY SUSPENDED, ie. WINTER SHUTDOWN, ALL DISTURBED AREAS MUST BE TEMPORARILY STABILIZED AND ONCE A MONTH INSPECTIONS CAN OCCUR. THE GENERAL PERMIT CAN BE REFERENCED FOR FURTHER INFORMATION.
- 20. ALL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED BY THE PROPERTY OWNER, AND SHALL BECOME THEIR RESPONSIBILITY.
- 21. PAVEMENT AREAS ARE TO BE KEPT CLEAN AT ALL TIMES.
- 22. DURING CONSTRUCTION, ANY ADDITIONAL CONTROL MEASURES DEEMED NECESSARY TO PREVENT EROSION OR CONTROL SEDIMENT BEYOND THESE MEASURES SHOWN ON THE APPROVED PLAN SHALL BE INSTALLED OR EMPLOYED AT THE DIRECTION OF THE PROJECT ENGINEER.
- 23. ALL TEMPORARY, STRUCTURAL EROSION AND SEDIMENT CONTROL MEASURES CAN BE REMOVED, WHEN ALL CONSTRUCTION ACTIVITY IDENTIFIED IN THE SWPPP HAS BEEN COMPLETED, ALL AREAS OF DISTURBANCE HAVE ACHIEVED FINAL STABILIZATION\*\* AND ALL POST-CONSTRUCTION STORMWATER MANAGEMENT PRACTICES HAVE BEEN CONSTRUCTED IN CONFORMANCE WITH THE SWPPP AND ARE OPERATIONAL.

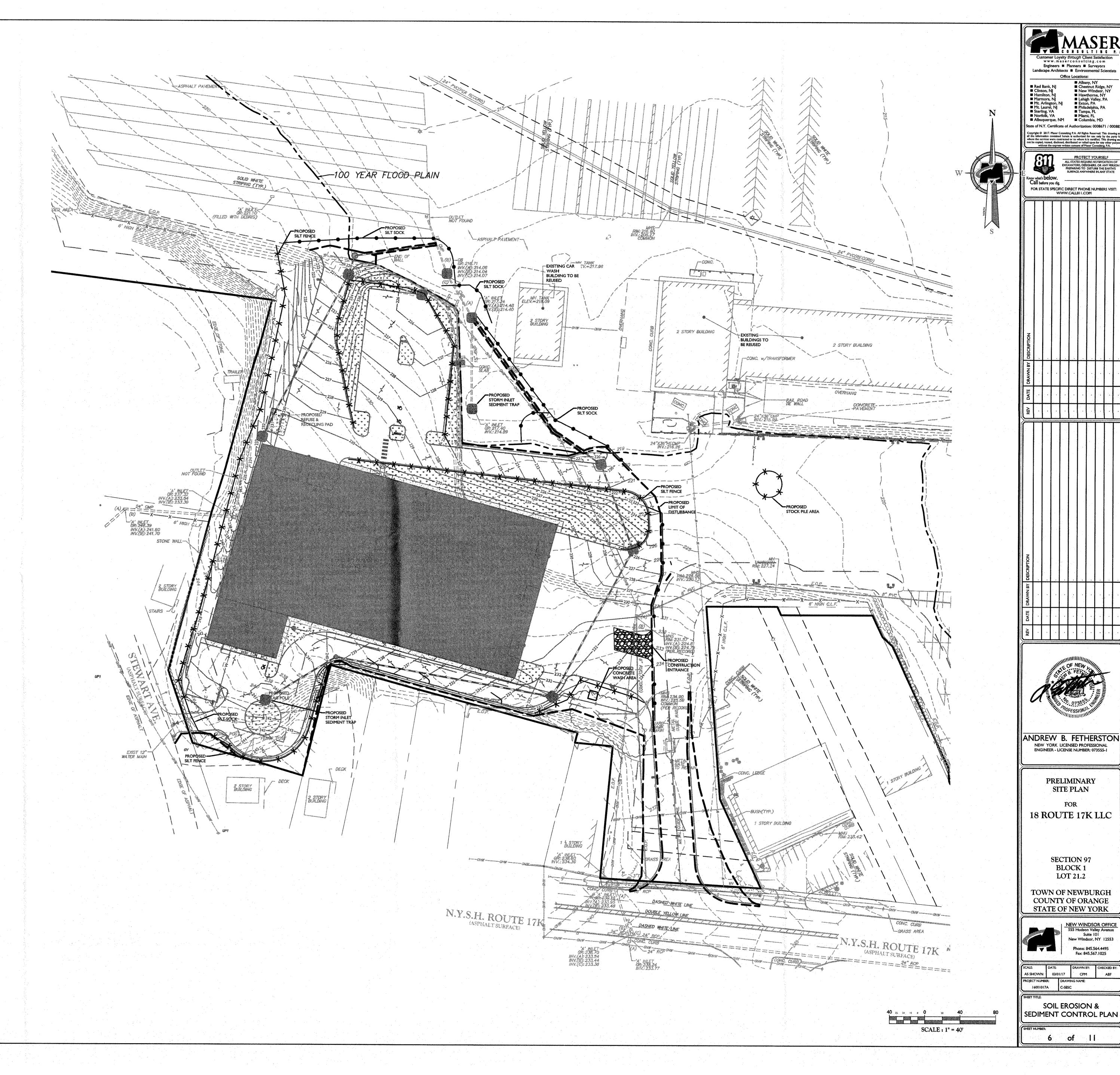
\*\*FINAL STABILIZATION - MEANS THAT ALL SOIL DISTURBANCE ACTIVITIES HAVE CEASED AND A UNIFORM, PERENNIAL VEGETATIVE COVER WITH A DENSITY OF EIGHTY (80) PERCENT OVER THE ENTIRE PERVIOUS SURFACE HAS BEEN ESTABLISHED; OR OTHER EQUIVALENT STABILIZATION MEASURES, SUCH AS PERMANENT LANDSCAPE MULCHES, ROCK RIP-RAP OR WASHED/CRUSHED STONE HAVE BEEN APPLIED ON ALL DISTURBED AREAS THAT ARE NOT COVERED BY PERMANENT STRUCTURES, CONCRETE OR PAVEMENT.

## MAINTENANCE PLAN DURING CONSTRUCTION:

INSPECTION AND MAINTENANCE SHALL BE PERFORMED IN CONFORMANCE WITH GP-0-15-002. ALL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CHECKED FOR STABILITY AND OPERATION FOLLOWING EVERY RUNOFF-PRODUCING RAINFALL BUT IN NO CASE LESS THAN ONCE EVERY WEEK. ANY NEEDED REPAIRS WILL BE MADE IMMEDIATELY TO MAINTAIN ALL PRACTICES AS DESIGNED AND INSTALLED FOR THE PROJECT. THE SEDIMENT TRAPS WILL BE CLEANED OUT WHEN THE LEVEL OF SEDIMENT REACHES 25% OF ITS' CAPACITY. SEDIMENT WILL BE REMOVED FROM BEHIND THE SILT FENCE WHEN IT BECOMES APPROXIMATELY 6" DEEP AT THE FENCE. THE SEDIMENT FENCE WILL BE REPAIRED AS NECESSARY TO MAINTAIN A BARRIER. ALL SEEDED AREAS WILL BE FERTILIZED, RE-SEEDED AS NECESSARY, AND MULCHED ACCORDING TO SPECIFICATIONS IN THE LANDSCAPE PLAN TO MAINTAIN A VIGOROUS, DENSE VEGETATIVE COVER.

NOTE: DURING THE CONSTRUCTION OF A PHASE, EACH SUBSEQUENT PHASE WILL HAVE BEEN CAPPED AND





Office Locations:

■ New Windsor, NY

Hawthorne, NY

■ Philadelphia, PA ■ Tampa, FL

ALL STATES RECORRE NOTIFICATION OF EXCAVATORS, DESIGNERS, OR ANY PERSON PREPARING TO DISTURB THE EARTH'S

BLOCK 1

LOT 21.2

Suite 101 New Windsor, NY 12553

Phone: 845.564.4495 Fax: 845,567,1025

CPM

THE APPLICANT AND THE APPLICANT'S CONTRACTOR ARE REQUIRED TO ATTEND A PRECONSTRUCTION MEETING WITH REPRESENTATIVES FROM THE TOWN BUILDING DEPARTMENT, HIGHWAY DEPARTMENT, ENGINEERS AND ANY OTHER PARTIES DEEMED NECESSARY TO REVIEW ALL PROTOCOLS, BONDING REQUIREMENTS, AGREEMENTS AND THE SEQUENCE AND SCHEDULING OF THE WORK BEING UNDERTAKEN, AS APPLICABLE.

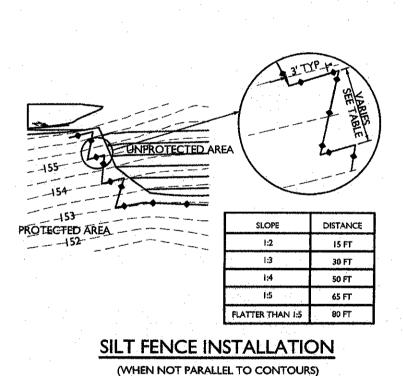
PHASE I (INCLUDES CONSTRUCTION OF A 66,100 SQ. FT. BUILDING AND ASSOCIATED SITE WORK IN THE SOUTHWEST CORNER OF THE EXISTING PARKING AREA):

DISTURBANCE AREA = 4.351 ACRES NOTE REGARDING TEMPORARY STORAGE: AS THIS PHASE OF CONSTRUCTION IS GENERALLY ENCOMPASSED BY IMPERVIOUS PARKING AREAS, TEMPORARY SEDIMENT BASIN STORAGE IS NEITHER PRACTICAL NOR APPLICABLE AS DISTURBANCE WILL BE AT GRADES BELOW THE SURROUNDING IMPERVIOUS AREAS. CARE SHOULD BE GIVEN TO GOOD HOUSEKEEPING TECHNIQUES AND LIMIT SEDIMENT TRACKING AS MUCH AS POSSIBLE.

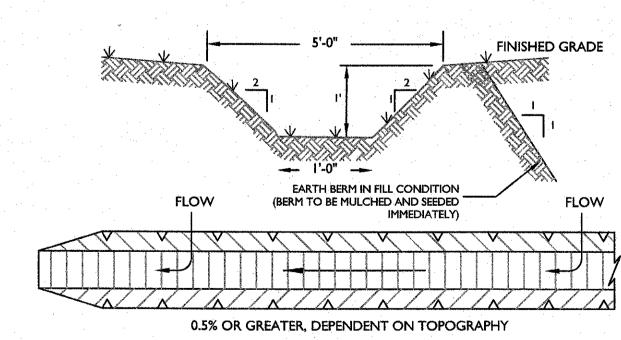
AREAS AS SHOWN WITHIN THIS PHASE AS SHOWN ON THE PLANS, USING CRUSHED STONE IN KIND WITH CONSTRUCTION ENTRANCE BEDDING AS SHOWN ON THE DETAIL.

I. CONSTRUCT AND MAINTAIN THE CONSTRUCTION ENTRANCE AND STAGING

- 2. CONTRACTOR SHALL INSTALL SILT SOCK AND SILT FENCE AS SHOWN ON THE EROSION AND SEDIMENT CONTROL PLAN.
- 3. SILT FENCE AND/OR SILT SOCK SHALL BE INSTALLED ALONG THE PROPOSED CONSTRUCTION LIMITS; SILT FENCE SHALL BE USED IN GRASS AREAS WHILE THE SILT SOCK SHALL BE USED ON IMPERVIOUS SURFACES.
- 4. CONTRACTOR SHALL SAW CUT EXISTING PAVEMENT ALONG CONSTRUCTION LIMIT AND REMOVE PAVEMENT AND SUBBASE AS APPLICABLE TO THE IMMEDIATE SCOPE, CAREFUL TO LIMIT EXPOSED SEDIMENT BELOW THE PAVEMENT SUBBASE.
- 5. CONTRACTOR SHALL EXCAVATE FOR BUILDING FOUNDATION, DRIVE AISLE, PARKING AREA AS SHOWN.
- 6. CONTRACTOR SHALL INSTALL PROPOSED UTILITIES INCLUDING STORM SEWER. WATER MAIN AND SERVICES, SANITARY SEWER SERVICES, DRAIN INLETS/CATCH BASINS. AND ANY SUBSURFACE UTILITIES IN ADVANCE OF INSTALLING CONCRETE CURB, SIDEWALK AND CURB ISLANDS.
- 7. CONTRACTOR SHALL IMPORT/EXPORT NECESSARY MATERIAL TO ROUGH GRADE SITE AND CONSTRUCT BUILDING PAD. NO FILL IS TO BE PLACED WITHIN THE LIMITS OF THE FLOODPLAIN. THE CONTRACTOR SHOULD CONTINUE WORK ON PROPOSED BUILDING AND AFTER COMPLETION OF BUILDING EXTERIOR, GRADE AND SPREAD TOPSOIL ON ALL LAWN AREAS AND SEED. MAINTAIN ALL SEEDED AND PLANTED AREAS TO INSURE A VIABLE STABILIZED VEGETATIVE COVER.
- 8. THE PROJECT SITE MUST MEET <u>FINAL STABILIZATION</u> CRITERIA PRIOR TO REMOVING ALL EROSION AND SEDIMENT CONTROL DEVICES AND CLOSING OUT THE PROJECT. LITTER AND CONSTRUCTION DEBRIS SHALL BE REMOVED AS PRACTICAL THROUGHOUT THE LIFE OF THE PROJECT.
- a. FINAL STABILIZATION MEANS THAT ALL SOIL DISTURBANCE ACTIVITIES HAVE CEASED AND A UNIFORM, PERENNIAL VEGETATIVE COVER WITH A DENSITY OF EIGHTY (80) PERCENT OVER THE ENTIRE PERVIOUS SURFACE HAS BEEN ESTABLISHED: OR OTHER EQUIVALENT STABILIZATION MEASURES, SUCH AS PERMANENT LANDSCAPE MULCHES, ROCK RIP-RAP OR WASHED/CRUSHED STONE HAVE BEEN APPLIED ON ALL DISTURBED AREAS THAT ARE NOT COVERED BY PERMANENT STRUCTURES, CONCRETE OR PAVEMENT.
- 9. THE GENERAL PERMIT FOR STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES STATES THAT IT IS UNLAWFUL FOR ANY PERSON TO CAUSE OR CONTRIBUTE TO A VIOLATION OF WATER QUALITY STANDARDS.
- 10.ADDITIONAL EROSION CONTROL MEASURES SHALL BE INSTALLED, AS MAY BE NECESSARY, REQUIRED AND/OR REQUESTED BY AUTHORITIES, TO PREVENT THE INCIDENTAL DISCHARGE OF SILT LADEN RUNOFF FROM ENTERING A WATER COURSE OR A DRAINAGE SYSTEM. THE GENERAL PERMIT FOR STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES STATES THAT IT IS UNLAWFUL FOR ANY PERSON TO CAUSE OR CONTRIBUTE TO A VIOLATION OF WATER QUALITY STANDARDS.
- II.PHASE I MUST BE STABILIZED PRIOR TO REMOVING ALL EROSION AND SEDIMENT CONTROL DEVICES AND PROGRESSING TO NEXT PHASE. LITTER AND CONSTRUCTION DEBRIS SHALL BE REMOVED AS PRACTICABLE PRIOR TO



N.T.S.



TO BE INSTALLED ABOVE DISTURBED AREAS, TO DIVERT RUNOFF OFF-SITE WITHOUT INCREASING EROSION; INTERMITTENTLY ACROSS DISTURBED AREAS, TO SHORTEN OVERLAND FLOW DISTANCES BELOW DISTURBED AREAS, TO DIVERT SEDIMENT-LADEN WATER TO A SEDIMENT TRAPPING DEVICE; AND TO SAFELY TRANSPORT RUNOFF ALONG ROADWAYS. \* WHERE SWALE TRAVERSES CONSTRUCTION VEHICLE PATH, DEPTH TO BE 6". SEE PLAN FOR

INSTALLATION NOTES ALL SWALES SHALL HAVE UNINTERRUPTED POSITIVE GRADE TO AN OUTLET.

STABILIZATION SHALL BE SEED AND STRAW MULCH.

- 2. DIVERTED RUNOFF FROM A DISTURBED AREA SHALL BE CONVEYED TO A SEDIMENT TRAPPING DEVICE. DIVERTED RUNOFF FROM AN UNDISTURBED AREA SHALL OUTLET DIRECTLY INTO AN UNDISTURBED STABILIZED AREA AT NON-EROSIVE VELOCITY.
- 4. ALL TREES, BRUSH, STUMPS, OBSTRUCTIONS AND OTHER OBJECTIONABLE MATERIAL SHALL BE REMOVED AND DISPOSED OF SO AS NOT TO INTERFERE WITH THE PROPER FUNCTIONING OF THE SWALE. 5. THE SWALE SHALL BE EXCAVATED OR SHAPED TO LINE, GRADE AND CROSS SECTION AS REQUIRED TO MEET THE CRITERIA SPECIFIED HEREIN AND BE FREE OF BANK PROJECTIONS OR OTHER IRREGULARITIES WHICH WILL
- IMPEDE NORMAL FLOW. 5. FILLS SHALL BE COMPACTED BY EARTH MOVING EQUIPMENT.
- ALL EARTH REMOVED AND NOT NEEDED FOR CONSTRUCTION SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE FUNCTIONING OF THE SWALE. INSPECTION AND MAINTENANCE MUST BE PROVIDED AFTER EACH RAIN EVENT.

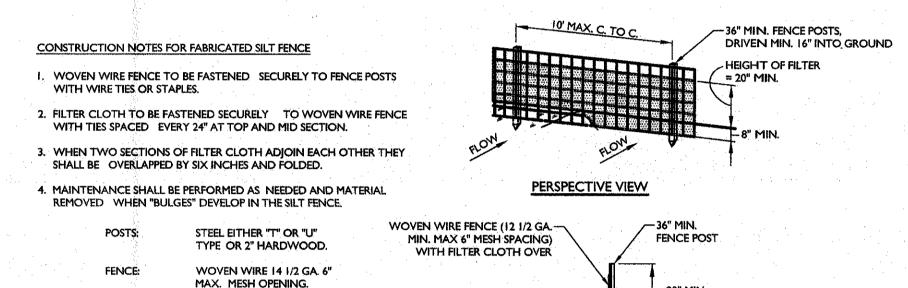
## GENERAL SEEDING NOTES

- I. TEMPORARY SEEDING SHALL CONSIST OF PERENNIAL RYEGRASS APPLIED AT A RATE OF 1.0 LBS. PER 1000 SF OR SPRING OATS APPLIED AT A RATE OF 2.0 LBS. PER 1000 SF. TEMPORARY SEEDING SHALL BE MULCHED AND MAINTAINED UNTIL DISTURBED AREAS ARE PERMANENTLY STABILIZED WITH PERMANENT SEEDING.
- 2. PERMANENT SEEDING SHALL CONSIST OF THE FOLLOWING MIXTURE OR APPROVED EQUAL OPTIMUM SEEDING DATES ARE BETWEEN APRIL I AND MAY 31: AND AUGUST 16 AND OCTOBER 15.

MIXTURE - HARD FESCUE 120 LBS/ACRE PERENNIAL RYE GRASS 30 LBS/ACRE KENTUCKY BLUE GRASS (BLEND) 40 LBS/ACRE MIXTURE - TALL FESCUE

160 LBS/ACRE PERENNIAL RYE GRASS (BLEND) 20 LBS/ACRE KENTUCKY BLUE GRASS (BLEND) 20 LBS/ACRE

- 3. PERMANENT SEEDING TO BE APPLIED BY RAKING OR DRILLING INTO THE SOILS AT A RATE OF 150# PER ACRE, SLOPED AREA TO BE COVERED WITH MULCH AS INDICATED IN NOTE 5.
- 4. FERTILIZER FOR THE ESTABLISHMENT OF TEMPORARY AND PERMANENT VEGETATIVE COVER SHALL BE IN COMPLIANCE WITH THE LATEST NYSDEC REGULATIONS. A SOIL TEST PRIOR TO FERTILIZER APPLICATION IS RECOMMENDED.
- 5. IF SEASON PREVENTS THE ESTABLISHMENT OF TEMPORARY OR PERMANENT SEEDING, EXPOSED AREA TO BE STABILIZED WITH MULCH AS INDICATED IN NOTE 6.
- 6. MULCH TO CONSIST OF SMALL GRAIN STRAW OR SALT HAY ANCHORED WITH A WOOD AND FIBER MULCH BINDER OR AN APPROVED EQUAL. MULCH WILL BE SPREAD AT RATES OF 90 TO 115 LBS. PER 1000 SF AND ANCHORED WITH A MULCH ANCHORING TOOL OR LIQUID MULCH BINDER, AND SHALL BE PROVIDED ON ALL SEEDINGS. HYDROMULCH SHALL ONLY BE USED DURING OPTIMUM GROWING SEASONS.
- 7. WORK LIME AND FERTILIZER INTO SOIL AS NEARLY AS PRACTICAL TO A DEPTH OF 4 INCHES WITH A DISC. SPRINGTOOTH HARROW, OR OTHER SUITABLE EQUIPMENT. THE FINAL HARROWING OR DISCING OPERATION SHOULD BE ON ON THE GENERAL CONTOUR. CONTINUE TILLAGE UNTILL A REASONABLY UNIFORM, FINE SEEDBED IS PREPARED. ALL BUT CLAY OR SILTY SOILS AND COARSE SANDS SHOULD BE ROLLED TO FIRM THE SEEDBED WHEREVER FEASIBLE.
- 8. REMOVE FROM THE SURFACE ALL STONES TWO INCHES OR LARGER IN ANY DIMENSION. REMOVE ALL OTHER DEBRIS, SUCH AS WIRE, CABLE, TREE ROOTS, PIECES OF CONCRETE, CLODS, LUMPS, OR OTHER UNSUITABLE MATERIAL.
- 9. INSPECT SEEDBED JUST BEFORE SEEDING. IF TRAFFIC HAS LEFT THE SOIL COMPACTED, THE AREA MUST BE RETILLED AND FIRMED AS ABOVE.



MIN. 6" INTO GROUND

#### WIRE REINFORCED SILT FENCE

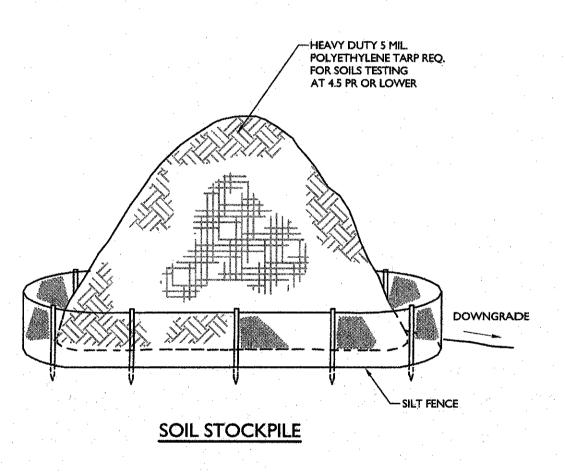
FILTER CLOTH: FILTER X, MIRAFI 100X.

STABILINKA TI40N, OR

APPROVED EQUAL.

ENVIROFENCE, OR

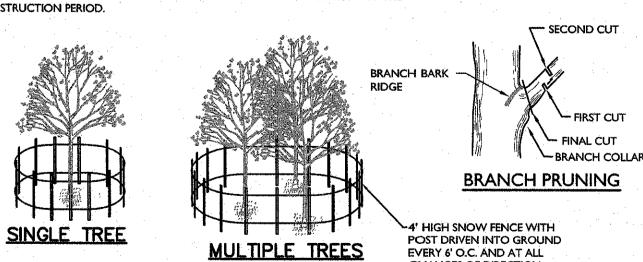
APPROVED EQUAL.



## TREE PROTECTION NOTES:

TREE PROTECTION FENCING TO BE INSTALLED ALONG THE DISTURBANCE LIMIT LINE HEREVER EROSION CONTROL FENCING IS NOT PROPOSED.

- 2. THERE SHALL BE NO STORAGE OF EQUIPMENT OR MATERIALS OUTSIDE CLEARING LIMITS. NO
- EQUIPMENT IS PERMITTED OUTSIDE CLEARING LIMITS. 3. THE FENCING SHOULD REMAIN IN GOOD CONDITION FOR THE DURATION OF THE



-UNDISTURBED

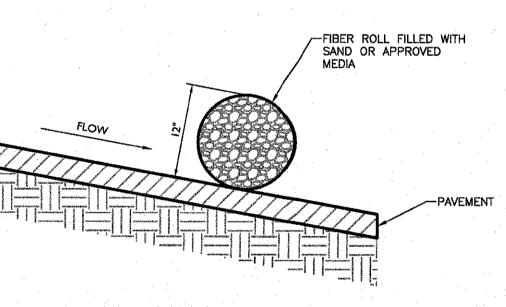
. PROTECTIVE FENCING IS TO BE ERECTED PRIOR TO CONSTRUCTION AND MAINTAINED DURING CONSTRUCTION 8. FEEDER ROOTS SHOULD NOT BE CUT IN AN AREA INSIDE THE DRIP LINE AS DIRECTED BY THE LANDSCAPE ARCHITECT, SOIL CONSERVATION DISTRICT AND/OR MUNICIPAL ENGINEER. 2. NO CONSTRUCTION ACTIVITY IS PERMITTED WITHIN THE PROTECTIVE FENCING. 3. AS CONSTRUCTION NEARS COMPLETION THE FENCING WILL BE REMOVED AS DIRECTED.

BUILDING OPERATIONS.

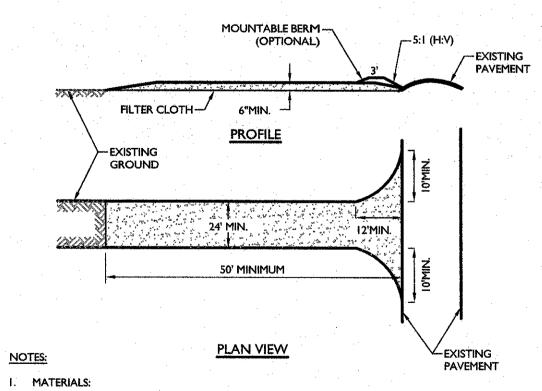
4. AT THE COMPLETION OF CONSTRUCTION ALL TREES WILL BE PRUNED AS NECESSARY TO CORRECT ANY DAMAGE RESULTING FROM CONSTRUCTION ACTIVITY. 5. GENERAL MECHANICAL DAMAGE - SEE DETAIL ABOVE FOR CORRECT PLACEMENT OF TREE PROTECTION. 6. BOX TREES WITHIN 25 FEET OF A BUILDING SITE TO PREVENT MECHANICAL INJURY, FENCING OR OTHER BARRIER SHOULD BE INSTALLED AT THE DRIP LINE OF THE TREE BRANCH COLLAR MAY LEAD TO EXCESS SPROUTING, CRACKS AND ROT. REMOVAL OF A "V" CROTCH SHOULD BE CONSIDERED FOR FREE STANDING SPECIMEN TREES TO AVOID FUTURE SPLITTING DAMAGE. 7. BOARDS WILL NOT BE NAILED TO TREES DURING

OF THE TREE BRANCHES 9. DAMAGED TRUNKS OR EXPOSED ROOTS SHOULD HAVE DAMAGED BARK REMOVED IMMEDIATELY AND NO PAINT SHALL BE APPLIED. EXPOSED ROOTS SHOULD BE COVERED WITH TOPSOIL IMMEDIATELY AFTER EXCAVATION IS COMPLETE. ROOTS SHALL BE PRUNED TO GIVE A CLEAN, SHARP SURFACE AMENABLE TO HEALING. ROOTS EXPOSED DURING HOT WEATHER SHOULD BE IRRIGATED TO PREVENT PERMANENT TREE INJURY. CARE FOR SERIOUS INJURY SHOULD BE PRESCRIBED BY A PROFESSIONAL FORESTER OR CERTIFIED TREE EXPERT. 10. TREE LIMB REMOVAL WHERE NECESSARY, WILL BE DONE AS NATURAL TARGET PRUNING TO REMOVE THE DESIRED BRANCH COLLAR. THERE SHOULD BE NO FLUSH CUTS. FLUSH CUTS DESTROY A MAJOR DEFENSE SYSTEM OF THE TREE, NO TREE PAINT SHALL BE APPLIED, ALL CUTS SHALL BE MADE AT THE OUTSIDE EDGE OF THE BRANCH COLLAR, CUTS MADE TOO FAR BEYOND THE

TEMPORARY TREE PROTECTION DETAIL



SILT SOCK INSTALLATION ON PAVEMENT



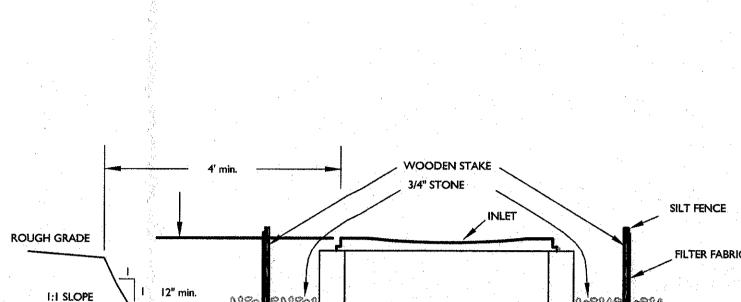
• STONE SIZE - USE 2" STONE, OR RECLAIMED/RECYCLED CONCRETE EQUIVALENT • LENGTH - NOT LESS THAN 50 FEET • THICKNESS - NOT LESS THAN (6) INCHES.

2. FILTER CLOTH - WILL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE. SURFACE WATER - ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED ACROSS THE ENTRANCE. IF PIPING IS IMPRACTICAL, A MOUNTING

BERM WITH 5:1 SLOPES WILL BE PERMITTED.

- 6. MAINTENANCE THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.
- WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE. 6. PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.

STABILIZED CONSTRUCTION ENTRANCE



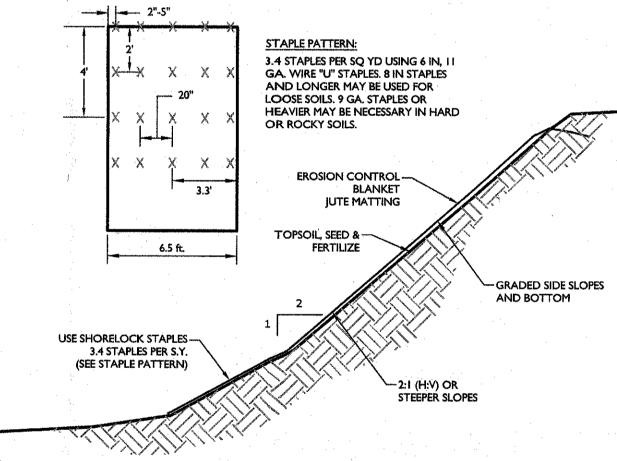
THIS PLAN IS FOR EROSION CONTRO

**PURPOSES ONLY** 

INSTALLATION NOTES 1. EXCAVATE A 6 INCH x 6 INCH TRENCH, OFFSET APPROXIMATELY 2 FEET FROM THE INLET

- 2. UNROLL A SECTION AT A TIME AND POSITION THE POSTS AGAINST THE BACK (DOWNSTREAM) WALL OF THE TRENCH (NET SIDE AWAY FROM DIRECTION OF FLOW). 3. DRIVE THE POST INTO THE GROUND UNTIL THE NETTING IS APPROXIMATELY 2 INCHES FROM THE TRENCH BOTTOM.
- 4. LAY THE TOE-IN FLAP OF FABRIC ONTO THE UNDISTURBED BOTTOM OF THE TRENCH. BACKFILL THE TRENCH AND TAMP THE SOIL STEEPER SLOPES REQUIRE AN INTERCEPT TRENCH. 5. JOIN SECTIONS AS SHOWN ABOVE. SUPPLEMENT WITH GRAVEL, PILED AGAINST THE FENCE.
- 6. SEDIMENT SHALL BE REMOVED AND THE TRAP RESTORED TO ITS ORIGINAL DIMENSIONS
- 7. THE STRUCTURE SHALL BE INSPECTED AFTER EACH RAIN AND REPAIRS MADE AS NEEDED.

# STORM INLET SEDIMENT TRAP



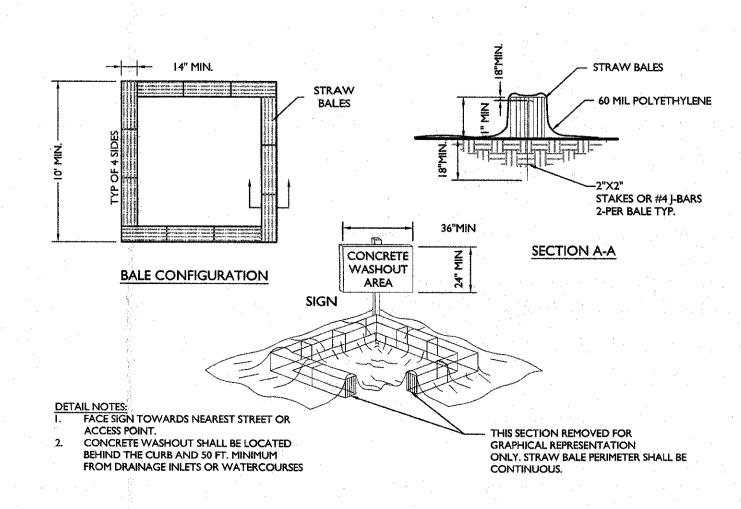
OR FLATTER

- PREPARE SOIL BEFORE INSTALLING BLANKETS, INCLUDING APPLICATION OF TOPSOIL, FERTILIZER,
- 2. BEGIN AT THE TOP OF THE SLOPE BY ANCHORING THE BLANKET IN A 6" DEEP x 6" WIDE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING.

8. EROSION CONTROL BLANKET SHALL BE JUTE MATTING.

- 3. ROLL BLANKETS IN DIRECTION OF WATER FLOW. 4. PLACE BLANKETS END OVER END (SHINGLE STYLE) WITH A 6" OVERLAP, USE A DOUBLE ROW OF
- STAGGERED STAPLES 4" APART TO SECURE BLANKETS, 5. FULL LENGTH EDGE OF BLANKETS AT TOP OF SIDE SLOPES MUST BE ANCHORED IN 6" (15 cm)
- DEEP X 6" (15 cm) WIDE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING.
- 6. BLANKETS ON SIDE SLOPES MUST BE OVERLAPPED 4" OVER THE CENTER BLANKET AND STAPLED 7. THE TERMINAL END OF THE BLANKETS MUST BE ANCHORED IN A 6" (15 cm) DEEP X 6" (15 cm) WIDE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING.

# SLOPE STABILIZATION DETAIL



**CONCRETE WASHOUT** 

■ Marmora, Nj ■ Mt. Arlington, ■ Mt. Laurel, N ■ Philadelphia, PA ■ Sterling, VA
■ Norfolk, VA Tampa, FL ■ Albuquerque, NM State of N.Y. Certificate of Authorization: 0008671 / 00088 Copyright © 2017. Maser Consulting P.A. All Rights Reserved. This drawing ar all the information contained herein is authorized for use only by the party for whom the services were contracted or to whom it is certified. This drawing not be copied, reused, disclosed, distributed or relied upon for any other purpor without the express written consent of Maser Consulting, P.A. ALL STATES REQUIRE NOTIFICATION OF EXCAVATORS, DESIGNERS, OR ANY PERSON PREPARING TO DISTURB THE EARTH'S Know what's **below.**Call before you dig FOR STATE SPECIFIC DIRECT PHONE NUMBERS VISIT WWW.CALL811.COM ANDREW B. FETHERSTON NEW YORK LICENSED PROFESSIONAL ENGINEER - LICENSE NUMBER: 073555-1

Engineers Planners Surveyors

Office Locations:

Clinton, NI

Landscape Architects 
Environmental Scientists

■ Chestnut Ridge, NY

■ New Windsor, NY

Hawthorne, N

**PRELIMINARY** SITE PLAN

8 ROUTE 17K LLC

SECTION 97 **BLOCK 1** 

TOWN OF NEWBURGH **COUNTY OF ORANGI** STATE OF NEW YORK

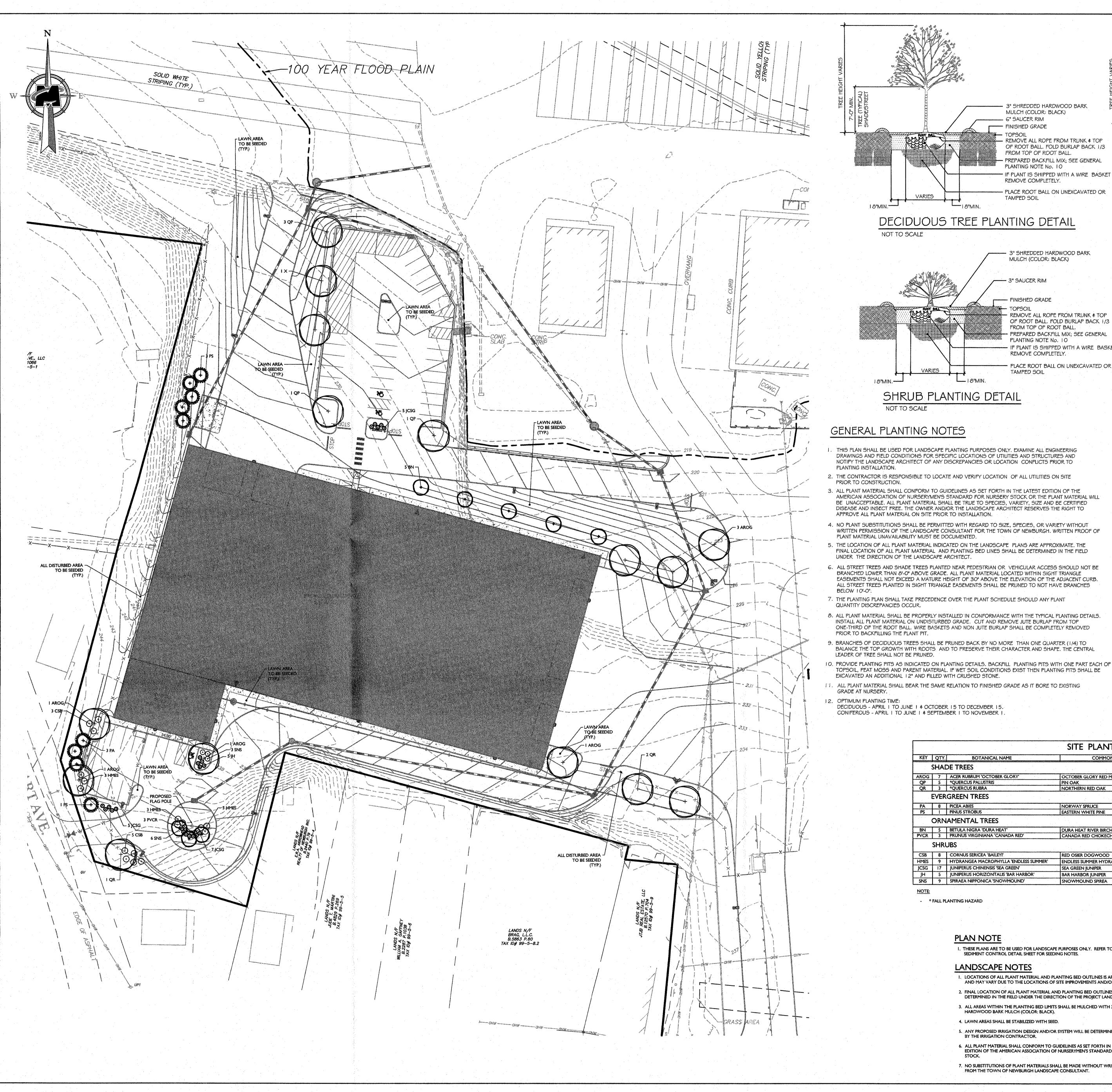
LOT 21.2

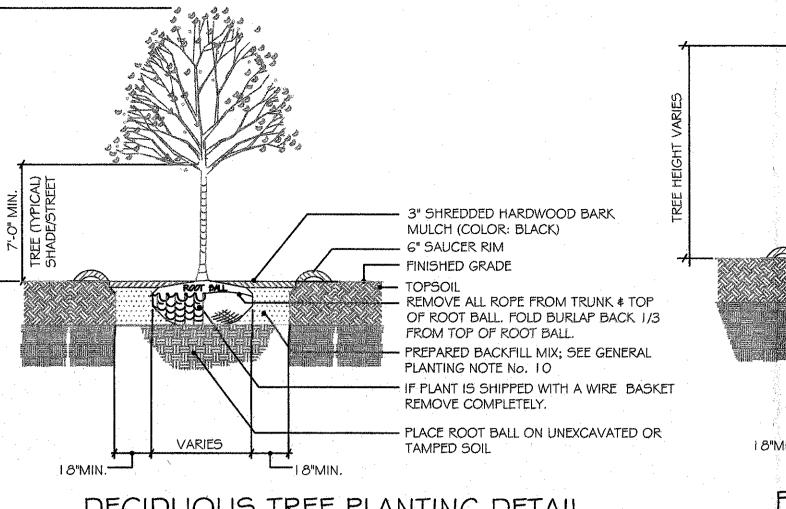
Suite 101

New Windsor, NY 12553 Phone: 845.564.4495 Fax: 845.567.1025

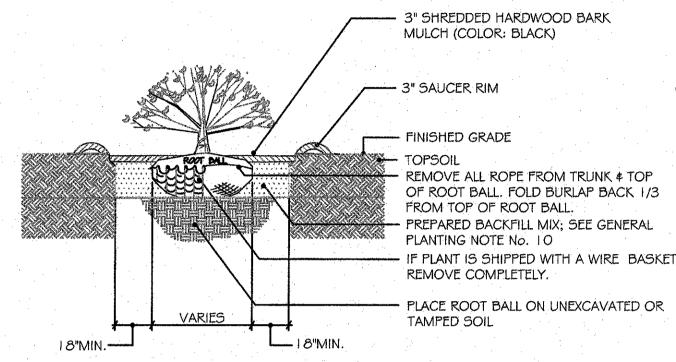
03/01/17 CPM 16001017A

SOIL EROSION & SEDIMENT CONTROL DETAILS





## DECIDUOUS TREE PLANTING DETAIL NOT TO SCALE



#### SHRUB PLANTING DETAIL NOT TO SCALE

#### GENERAL PLANTING NOTES

- THIS PLAN SHALL BE USED FOR LANDSCAPE PLANTING PURPOSES ONLY. EXAMINE ALL ENGINEERING DRAWINGS AND FIELD CONDITIONS FOR SPECIFIC LOCATIONS OF UTILITIES AND STRUCTURES AND NOTIFY THE LANDSCAPE ARCHITECT OF ANY DISCREPANCIES OR LOCATION CONFLICTS PRIOR TO
- 2. THE CONTRACTOR IS RESPONSIBLE TO LOCATE AND VERIFY LOCATION OF ALL UTILITIES ON SITE
- 3. ALL PLANT MATERIAL SHALL CONFORM TO GUIDELINES AS SET FORTH IN THE LATEST EDITION OF THE AMERICAN ASSOCIATION OF NURSERYMEN'S STANDARD FOR NURSERY STOCK OR THE PLANT MATERIAL WILL BE UNACCEPTABLE. ALL PLANT MATERIAL SHALL BE TRUE TO SPECIES, VARIETY, SIZE AND BE CERTIFIED DISEASE AND INSECT FREE. THE OWNER AND/OR THE LANDSCAPE ARCHITECT RESERVES THE RIGHT TO APPROVE ALL PLANT MATERIAL ON SITE PRIOR TO INSTALLATION.
- 4. NO PLANT SUBSTITUTIONS SHALL BE PERMITTED WITH REGARD TO SIZE, SPECIES, OR VARIETY WITHOUT WRITTEN PERMISSION OF THE LANDSCAPE CONSULTANT FOR THE TOWN OF NEWBURGH. WRITTEN PROOF OF PLANT MATERIAL UNAVAILABILITY MUST BE DOCUMENTED
- 5. THE LOCATION OF ALL PLANT MATERIAL INDICATED ON THE LANDSCAPE PLANS ARE APPROXIMATE. THE FINAL LOCATION OF ALL PLANT MATERIAL AND PLANTING BED LINES SHALL BE DETERMINED IN THE FIELD UNDER THE DIRECTION OF THE LANDSCAPE ARCHITECT.
- S. ALL STREET TREES AND SHADE TREES PLANTED NEAR PEDESTRIAN OR VEHICULAR ACCESS SHOULD NOT BE BRANCHED LOWER THAN 8'-O" ABOVE GRADE. ALL PLANT MATERIAL LOCATED WITHIN SIGHT TRIANGLE EASEMENTS SHALL NOT EXCEED A MATURE HEIGHT OF 30" ABOVE THE ELEVATION OF THE ADJACENT CURB. ALL STREET TREES PLANTED IN SIGHT TRIANGLE EASEMENTS SHALL BE PRUNED TO NOT HAVE BRANCHES
- 7. THE PLANTING PLAN SHALL TAKE PRECEDENCE OVER THE PLANT SCHEDULE SHOULD ANY PLANT QUANTITY DISCREPANCIES OCCUR.
- 8. ALL PLANT MATERIAL SHALL BE PROPERLY INSTALLED IN CONFORMANCE WITH THE TYPICAL PLANTING DETAILS. INSTALL ALL PLANT MATERIAL ON UNDISTURBED GRADE. CUT AND REMOVE JUTE BURLAP FROM TOP ONE-THIRD OF THE ROOT BALL. WIRE BASKETS AND NON JUTE BURLAP SHALL BE COMPLETELY REMOVED PRIOR TO BACKFILLING THE PLANT PIT.
- 9. BRANCHES OF DECIDUOUS TREES SHALL BE PRUNED BACK BY NO MORE THAN ONE QUARTER (1/4) TO BALANCE THE TOP GROWTH WITH ROOTS AND TO PRESERVE THEIR CHARACTER AND SHAPE. THE CENTRAL LEADER OF TREE SHALL NOT BE PRUNED.
- TOPSOIL, PEAT MOSS AND PARENT MATERIAL. IF WET SOIL CONDITIONS EXIST THEN PLANTING PITS SHALL BE EXCAVATED AN ADDITIONAL 12" AND FILLED WITH CRUSHED STONE. ALL PLANT MATERIAL SHALL BEAR THE SAME RELATION TO FINISHED GRADE AS IT BORE TO EXISTING
- GRADE AT NURSERY. 12. OPTIMUM PLANTING TIME:
- DECIDUOUS APRIL 1 TO JUNE 1 # OCTOBER 15 TO DECEMBER 15. CONIFEROUS - APRIL | TO JUNE | & SEPTEMBER | TO NOVEMBER |.

--- 3" SHREDDED HARDWOOD BARK MULCH (COLOR: BLACK) ——6" SAUCER RIM - REMOVE ALL ROPE FROM TRUNK # TOP OF ROOT BALL. FOLD BURLAP BACK 1/3 FROM TOP OF ROOT BALL. PREPARED BACKFILL MIX; SEE GENERAL PLANTING NOTE No. 10 IF PLANT IS SHIPPED WITH A WIRE BASKET REMOVE COMPLETELY. PLACE ROOT BALL ON UNEXCAVATED OR TAMPED SOIL

# EVERGREEN TREE PLANTING DETAIL

NOT TO SCALE

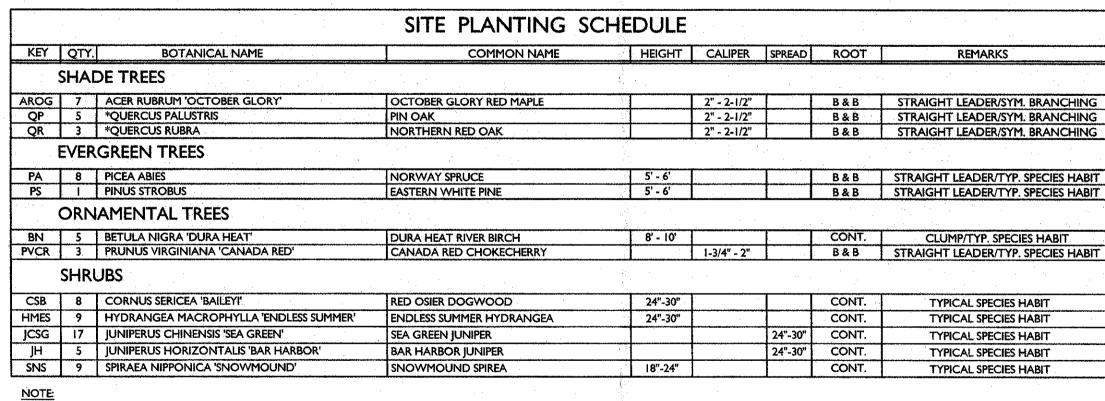
#### PLANT DETAIL NOTES

THE ADDITION OF LOOSE AGGREGATE.

- 1. NO SOIL OR MULCH SHALL BE PLACED AGAINST ROOT COLLAR OF PLANT. MULCH SHALL NOT TOUCH
- 2. PLANTING DEPTH SHALL BE THE SAME OR HIGHER AS GROWN IN NURSERY.
- 3. WIRE BASKETS AND NON-JUTE BURLAP MUST BE ENTIRELY REMOVED FROM THE ROOT BALL. JUTE BURLAP MUST BE REMOVED FROM THE TOP 1/3 OF THE ROOT BALL.
- 4. DEPTH OF PLANT PIT SHALL BE INCREASED BY 12" WHEREVER POOR SOIL CONDITIONS OCCUR, WITH
- 5. CONTRACTOR SHALL PARTIALLY FILL WITH WATER A REPRESENTATIVE NUMBER OF PITS IN EACH AREA OF THE PROJECT PRIOR TO PLANTING TO DETERMINE IF THERE IS ADEQUATE PERCOLATION. IF PIT
- 6. PLANTING MUST BE GUARANTEED FOR TWO FULL GROWING SEASONS FROM THE TIME OF FINAL ACCEPTANCE BY THE LANDSCAPE CONSULTANT. CONTRACTOR SHALL REMOVE ALL WRAPPING AT THE
- END OF GUARANTEE PERIOD. 7. BACKFILL MIXTURE TO BE SPECIFIED BASED UPON SOIL TEST AND CULTURAL REQUIREMENTS OF PLANT

DOESN'T PERCOLATE, MEASURES MUST BE TAKEN TO ASSURE PROPER DRAINAGE BEFORE PLANTING.

- 8. PRUNE DAMAGED AND CONFLICTING BRANCHES MAINTAINING NORMAL TREE SHAPE, NEVER CUT CENTRAL TRUNK OR LEADER.
- 13. NEWLY INSTALLED PLANT MATERIAL SHALL BE WATERED AT THE TIME OF INSTALLATION. REGULAR WATERING SHALL BE PROVIDED TO ENSURE THE ESTABLISHMENT, GROWTH AND SURVIVAL OF ALL PLANTS.
- 14. ALL PLANT MATERIAL SHALL BE GUARANTEED FOR TWO YEARS AFTER THE DATE OF FINAL ACCEPTANCE. ANY PLANT MATERIAL THAT DIES WITHIN THAT TIME PERIOD SHALL BE REMOVED, INCLUDING THE STUMP, AND REPLACED BY A TREE OF SIMILAR SIZE AND SPECIES AT NO EXPENSE TO THE OWNER.
- 15. THE LANDSCAPE CONTRACTOR SHALL PROVIDE A MINIMUM 4"-6" LAYER OF TOPSOIL IN ALL LAWN AREAS AND A MINIMUM OF 12" OF TOPSOIL IN ALL PLANTING AREAS, A FULL SOIL ANALYSIS SHALL BE CONDUCTED AFTER CONSTRUCTION AND PRIOR TO PLANTING TO DETERMINE THE EXTENT OF SOIL AMENDMENT REQUIRED. SOIL PH SHOULD BE 5.5-6.5.
- 16. ALL DISTURBED LAWN AREAS SHALL BE STABILIZED WITH SEED AS INDICATED ON THE LANDSCAPE PLANS. TEMPORARY SEEDING SHALL BE IN ACCORDANCE WITH THE SOIL EROSION AND SEDIMENT CONTROL PLAN. WHILE PERMANENT SEEDING SHALL BE IN ACCORDANCE WITH THE SEEDING NOTES ON THE EROSION CONTROL SHEET. ALL DISTURBED LAWN AREAS SHALL BE TOPSOILED, LIMED, FERTILIZED AND FINE GRADED PRIOR TO LAWN INSTALLATION.
- 17. ALL PLANTING BEDS SHALL RECEIVE 3" OF SHREDDED HARDWOOD BARK MULCH (COLOR: BLACK).
- 18. ALL SHRUB MASSES SHALL BE PLANTED IN CONTINUOUS MULCHED BEDS.
- 19. ALL EXISTING TREES AND SHRUBS TO BE PRESERVED ON SITE SHALL BE PROTECTED AGAINST CONSTRUCTION DAMAGE BY SNOW FENCING. ALL FENCING SHALL BE PLACED OUTSIDE THE INDIVIDUAL TREE CANOPY. ALL TREES TO REMAIN SHALL BE IDENTIFIED IN THE FIELD PRIOR TO COMMENCEMENT OF CONSTRUCTION, TREE PROTECTION FENCING SHALL BE INSTALLED PRIOR TO COMMENCEMENT OF CONSTRUCTION, GRADING OR CLEARING. ALL EXISTING VEGETATION BEING PRESERVED AND LOCATED AT THE EDGE OF THE NEW TREELINE, SHALL BE PRUNED AND TRIMMED TO REMOVE ALL DEAD, DISEASED, OR DAMAGED BRANCHES.
- 20. ALL PLANTING DEBRIS (WIRE, TWINE, RUBBER HOSE, BACKFILL ETC.) SHALL BE REMOVED FROM THE SITE AFTER PLANTING IS COMPLETE. PROPERTY IS TO BE LEFT IN A NEAT ORDERLY CONDITION IN ACCORDANCE WITH ACCEPTED PLANTING PRACTICES.



\* FALL PLANTING HAZARD

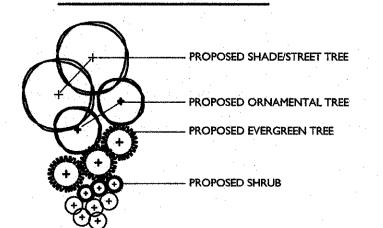
## PLAN NOTE

I. THESE PLANS ARE TO BE USED FOR LANDSCAPE PURPOSES ONLY. REFER TO EROSION ANS SEDIMENT CONTROL DETAIL SHEET FOR SEEDING NOTES.

## LANDSCAPE NOTES

- I. LOCATIONS OF ALL PLANT MATERIAL AND PLANTING BED OUTLINES IS APPROXIMATE AND MAY VARY DUE TO THE LOCATIONS OF SITE IMPROVEMENTS AND/OR BUILDINGS.
- 2. FINAL LOCATION OF ALL PLANT MATERIAL AND PLANTING BED OUTLINES SHALL BE DETERMINED IN THE FIELD UNDER THE DIRECTION OF THE PROJECT LANDSCAPE ARCHITECT.
- 3. ALL AREAS WITHIN THE PLANTING BED LIMITS SHALL BE MULCHED WITH 3" OF SHREDDED HARDWOOD BARK MULCH (COLOR: BLACK). 4. LAWN AREAS SHALL BE STABILIZED WITH SEED.
- 5. ANY PROPOSED IRRIGATION DESIGN AND/OR SYSTEM WILL BE DETERMINED IN THE FIELD BY THE IRRIGATION CONTRACTOR.
- 6. ALL PLANT MATERIAL SHALL CONFORM TO GUIDELINES AS SET FORTH IN THE LATEST EDITION OF THE AMERICAN ASSOCIATION OF NURSERYMEN'S STANDARD FOR NURSERY
- 7. NO SUBSTITUTIONS OF PLANT MATERIALS SHALL BE MADE WITHOUT WRITTEN CONSENT FROM THE TOWN OF NEWBURGH LANDSCAPE CONSULTANT.

## LANDSCAPE LEGEND



30 24 18 12 6 0 15 30 SCALE: 1'' = 30'

Engineers # Planners # Surveyors Landscape Architects Environmental Scientists Red Bank, N
Clinton, N
Hamilton, N
Hamilton, N
Marmora, N
Mt. Arlingto
Mt. Laurel, N
Sterling, VA
Norfolk, VA ■ Hawthorne, N Lehigh Valley, PA
Exton, PA
Philadelphia, PA
Tampa, FL without the express written consent of Maser Consulting, P.A.

ALL STATES REQUIRE NOTIFICATION OF EXCAVATORS, DESIGNERS, OR ANY PERSON PREPARING TO DISTURB THE EARTH'S SURFACE ANYWHERE IN ANY STATE Call before you dig. FOR STATE SPECIFIC DIRECT PHONE NUMBERS VISIT WWW.CALL811.COM



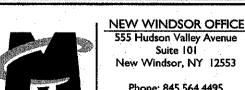
JUSTIN E. DATES NEW YORK REGISTERED ANDSCAPE ARCHITECT - LICENSE NUMBER: 001964

> **PRELIMINARY** SITE PLAN

18 ROUTE 17K LLC

**SECTION 97** BLOCK 1 LOT 21.2

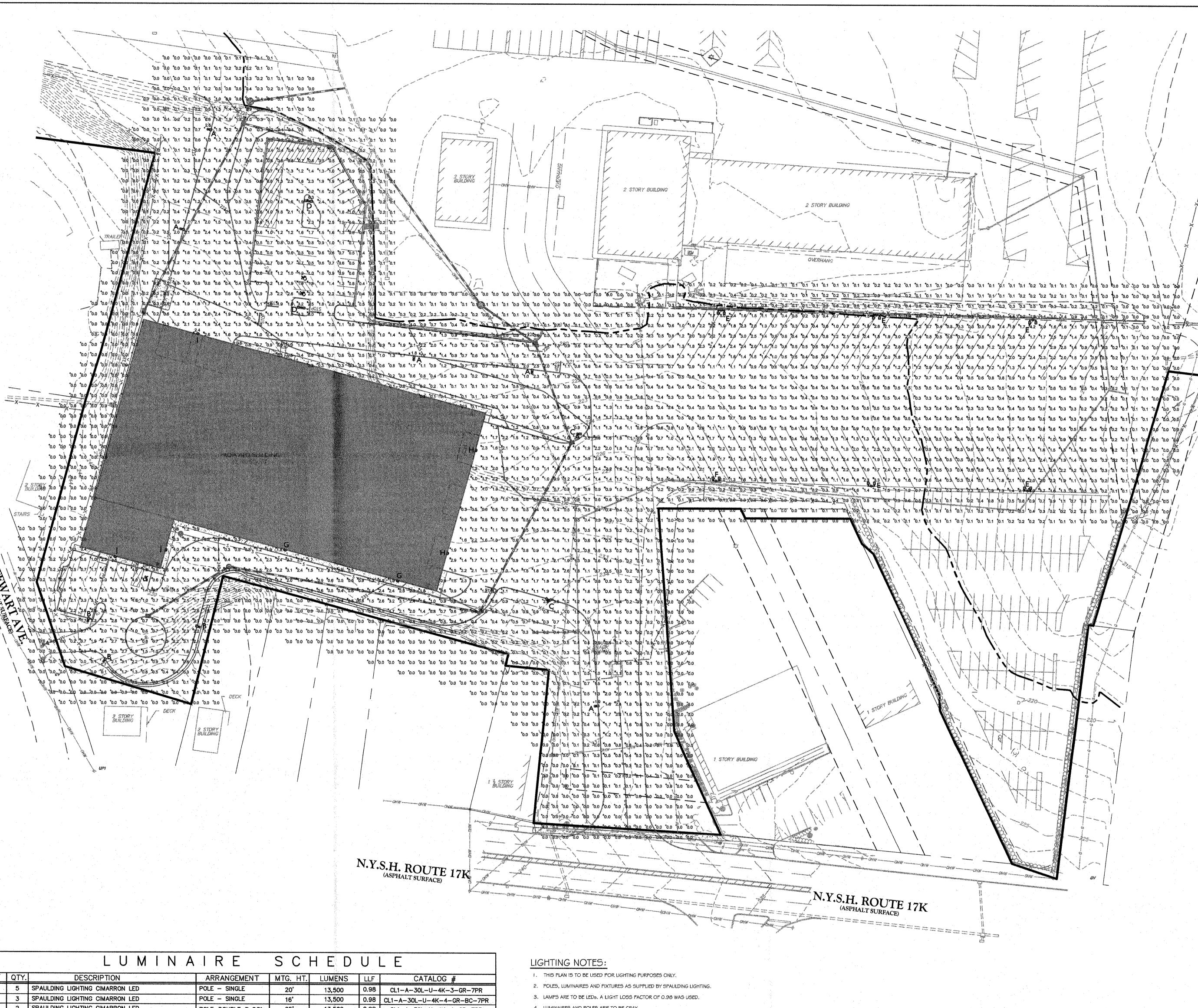
TOWN OF NEWBURGH COUNTY OF ORANGE STATE OF NEW YORK



New Windsor, NY 12553 Phone: 845.564.4495 Fax: 845.567.1025

AS SHOWN 03/01/17 CM DRAWING NAME: 16001017A

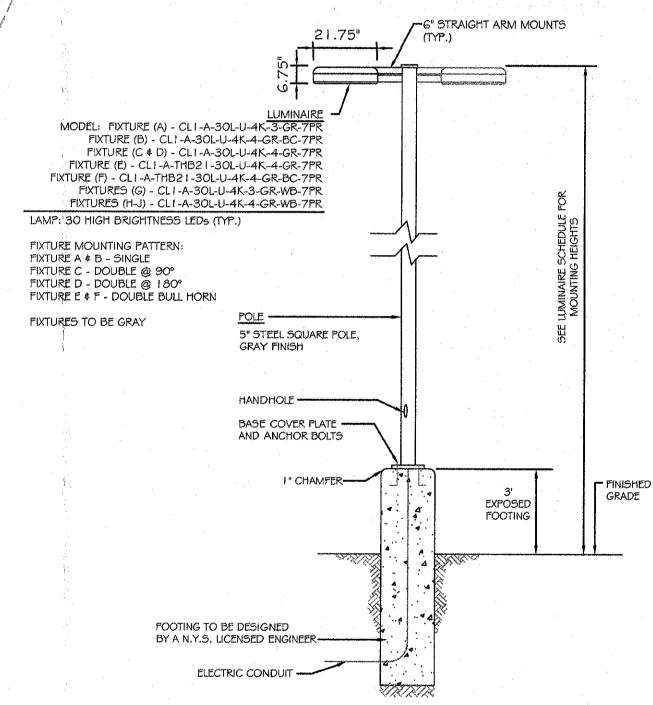
LANDSCAPE PLAN



LUMIN		N A	IRE	SCI	HED	ULE		
KEY	QTY.	DESCRIPTION		ARRANGEMENT	MTG. HT.	LUMENS	LLF	CATALOG #
Α	5	SPAULDING LIGHTING CIMARRON LED		POLE - SINGLE	20'	13,500	0.98	CL1-A-30L-U-4K-3-GR-7PR
В	3	SPAULDING LIGHTING CIMARRON LED		POLE - SINGLE	16'	13,500	0.98	CL1-A-30L-U-4K-4-GR-BC-7PR
С	2	SPAULDING LIGHTING CIMARRON LED		POLE-DOUBLE @ 90'	25'	13,500	0.98	CL1-A-30L-U-4K-4-GR-7PR
D	2	SPAULDING LIGHTING CIMARRON LED		POLE-DOUBLE @ 180°	20'	13,500	0.98	CL1-A-30L-U-4K-4-GR-7PR
E	5	SPAULDING LIGHTING CIMARRON LED		POLE-DOUBLE BULL HORN	25'	13,500	0.98	CL1-A-THB21-30L-U-4K-4-GR-7PR
F	1	SPAULDING LIGHTING CIMARRON LED		POLE-DOUBLE BULL HORN	25'	13,500	0.98	CL1-A-THB21-30L-U-4K-4-GR-7PR
G	2	SPAULDING LIGHTING CIMARRON LED		WALL MOUNT-SINGLE	17'	13,500	0.98	CL1-A-30L-U-4K-3-GR-WB-7PR
Н	2	SPAULDING LIGHTING CIMARRON LED		WALL MOUNT-SINGLE	20'	13,500	0.98	CL1-A-30L-U-4K-4-GR-WB-7PR
	1	SPAULDING LIGHTING CIMARRON LED		WALL MOUNT-SINGLE	17'	13,500	0.98	CL1-A-30L-U-4K-4-GR-WB-7PR
J	2	SPAULDING LIGHTING CIMARRON LED		WALL MOUNT-SINGLE	12'	13,500	0.98	CL1-A-30L-U-4K-4-GR-WB-7PR

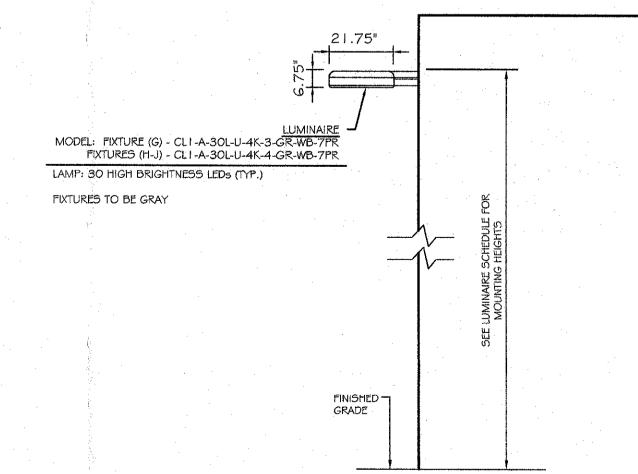
CALCULATION SUMMARY					
AREA	CALC TYPE	UNITS	AVG	MAX	MIN
PARKING/DRIVES/WALKS	ILLUMINANCE	Fc	1.1	6.3	0.1
					L

- 4. LUMINAIRES AND POLES ARE TO BE GRAY.
- 5. POLE MOUNTED FIXTURES SHALL BE PLACED A MINIMUM OF THREE (3) FEET BEHIND CURBS IN
- 6. PROPOSED LIGHT FIXTURE LOCATIONS ARE CRITICAL TO PROVIDE THE LIGHTING LEVELS DEPICTED ON THIS PLAN. THE LIGHTING CONTRACTOR SHALL FIELD VERIFY FIXTURE LOCATIONS PRIOR TO INSTALLATION. IF ADJUSTMENT TO ANY LIGHT FIXTURE LOCATION IS REQUIRED DUE TO FINAL CONSTRUCTION OF UTILITIES AND SITE IMPROVEMENTS, THE LIGHTING CONTRACTOR SHALL NOTIFY THE PROJECT LANDSCAPE ARCHITECT OF ANY DISCREPANCIES PRIOR TO INSTALLATION.
- 7. POLE SIZES TO BE CONFIRMED WITH MANUFACTURER.
- 8. LIGHTING SHOWN ON PLAN DEPICTS AVERAGE MAINTAINED FOOTCANDLE LEVELS AT GRADE. 9. CONTRACTOR TO PROVIDE SHOP DRAWINGS OF LIGHT FIXTURES FOR REVIEW AND APPROVAL BY
- THE PROJECT LANDSCAPE ARCHITECT.
- 10. ELECTRICAL PLANS FOR WIRING LAYOUT BY OTHERS.
- II. POLE BASE INSTALLATION SHALL INCLUDE A SUPPLEMENTARY GROUND ROD AND WIRE LEAD TO BASE FOR POWER CONNECTION. DETAILS PER PROJECT ELECTRICAL ENGINEER.



#### POLE MOUNTED FIXTURE DETAIL (FIXTURES: A-F) NOT TO SCALE

- NOTES: 1. LUMINAIRES AND POLE TO BE MANUFACTURED BY SPAULDING LIGHTING.
- 2. CONTRACTOR TO PROVIDE SHOP DRAWINGS OF LIGHT FIXTURES FOR REVIEW AND APPROVAL BY THE OWNER OR PROJECT LANDSCAPE ARCHITECT
- 3. FOOTING TO BE DESIGNED, SIGNED, AND SEALED BY A N.Y.S. LICENSED ENGINEER.
- 4. VOLTAGE TO BE CONFIRMED BY ELECTRICIAN CONTRACTOR



# WALL MOUNTED FIXTURE DETAIL (FIXTURES:G-J)

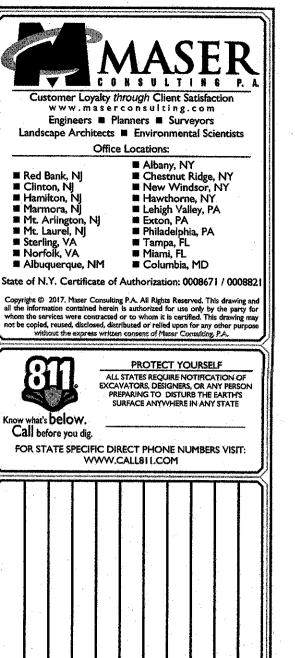
NOT TO SCALE NOTES:

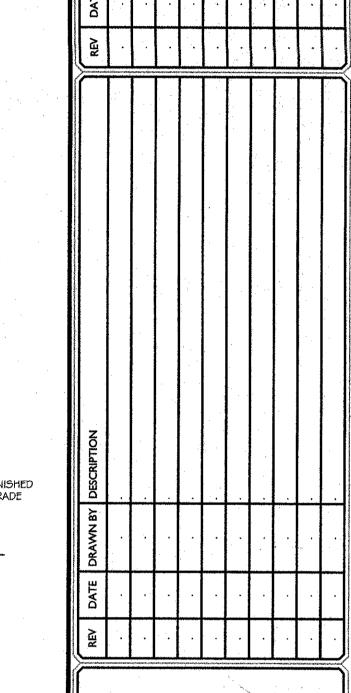
1. LUMINAIRES AND POLE TO BE MANUFACTURED BY SPAULDING LIGHTING. 2. CONTRACTOR TO PROVIDE SHOP DRAWINGS OF LIGHT FIXTURES FOR REVIEW AND

APPROVAL BY THE OWNER OR PROJECT LANDSCAPE ARCHITECT. 3. WALL BRACKET CATALOG NUMBER: WB-AREA-DB

4. VOLTAGE TO BE CONFIRMED BY ELECTRICIAN CONTRACTOR.

40 32 24 16 8 0 20 40 SCALE: 1" = 40'







JUSTIN E. DATES NEW YORK REGISTERED ANDSCAPE ARCHITECT - LICENSE NUMBER: 001964

> **PRELIMINARY** SITE PLAN

18 ROUTE 17K LLC

**SECTION 97** BLOCK 1 LOT 21.2

TOWN OF NEWBURGH **COUNTY OF ORANGE** STATE OF NEW YORK



Suite 101 New Windsor, NY 12553 Phone: 845,564,4495 Fax: 845.567.1025

LIGHTING PLAN

